

**IN THE UNITED STATES DISTRICT COURT  
FOR THE WESTERN DISTRICT OF PENNSYLVANIA**

VOICES FOR INDEPENDENCE, (VFI), )  
on behalf of themselves and all others )  
similarly situated )

Plaintiffs, )

v. )

Civil action No.: 1:06cv00078

COMMONWEALTH OF PENNSYLVANIA )  
DEPARTMENT OF TRANSPORTATION; )  
ALLEN D. BIEHLER, P.E., in his official )  
capacity as Secretary of Transportation of )  
the Commonwealth of Pennsylvania, CITY OF )  
ERIE, PENNSYLVANIA and CITY OF )  
MEADVILLE, PENNSYLVANIA )

Defendants. )

HON: SEAN J. McLAUGHLIN

MAG. JUDGE BAXTER

**FIFTH SETTLEMENT AGREEMENT BETWEEN PLAINTIFFS AND DEFENDANT  
COMMONWEALTH OF PENNSYLVANIA DEPARTMENT OF TRANSPORTATION**

**I. BACKGROUND**

On March 28, 2006, Plaintiffs filed a class action complaint against the Commonwealth of Pennsylvania Department of Transportation (“PennDOT”) alleging violations of Title II of the Americans with Disabilities Act, and of the Rehabilitation Act of 1973, arising from PennDOT’s failure to comply with these laws, and their implementing regulations, when PennDOT installed curb ramps along PennDOT highways located in the Cities of Erie and Meadville, Pennsylvania.

The Court previously has approved and has expressly retained jurisdiction over the parties to interpret and to enforce the terms of four Settlement Agreements between Plaintiffs and PennDOT. See (Doc. # 23), entered December 22, 2006; (Doc. # 33), entered April 19, 2007; (Doc. # 51), entered March 4, 2009; and (Doc. # 56), entered January 30, 2012.

During the last year, the parties have litigated and/or negotiated pursuant to the Fourth Settlement Agreement (Doc. # 56). On January 29, 2013, the Court ruled on some of these matters. See Minute Entry, January 29, 2013. The parties have agreed to the terms of an additional Settlement Order, the Court has considered these, and the Court now finds as follows:

## II. AGREEMENT

**A. Earlier Settlement Agreements Remain in Effect.** The requirements of the Parties' earlier Settlement Agreement Orders (Docs. # 23, 33, 51 and 56) remain in full force and effect, except to the extent expressly set forth below.

### **B. Specific Locations and Curb Ramps to Be Installed.**

1). **PennDOT's Listing of ramps to be installed.** The Parties attach hereto the "PennDOT Curb Ramp Inventory for the City of Erie," see Attachment A, and the "PennDOT Curb Ramp Inventory for the City of Meadville," see Attachment B. These Inventories were prepared by PennDOT to provide the information required by the Fourth Settlement Agreement (Doc. # 56), at Section II(C)(1), respecting curb ramps that had been omitted by PennDOT during previous repaving work. These listings identify the location and the number of curb ramps that PennDOT has agreed to install. Accordingly, PennDOT shall install each curb ramp that is listed on each inventory in accordance with the provisions contained in the prior settlement agreements between the Parties.

2). **Deadline.** PennDOT shall ensure that each listed curb ramp shall be installed no later than the end of year 2013 (December 31, 2013). However, PennDOT has advised Plaintiffs and Plaintiffs are aware that the PennDOT Curb Ramp Inventory for the City of Meadville includes the SR 0027 (North Street) A02 Project in the City of Meadville (found at Attachment B, at Lines 6 to 12). This is a major road reconstruction project which is currently under contract and scheduled for

two (2) years, beginning in 2013 and ending in 2014. Consequently, approximately thirty (30) curb ramps in this project are scheduled for construction in 2014.

**C. Replacement of Certain Curb Ramps Built From Year 2009 Through 2012.**

Previously, PENNDOT constructed non-compliant curb ramps in the City of Meadville and the City of Erie with portions of cross slopes exceeding two (2) percent. These curb ramps, which are identified below, will be replaced in accordance with the provisions contained in the prior settlement agreements, they are as follows:

In the City of Meadville, the following non-compliant curb ramp locations will be removed and replaced:

- SR 2034 (Spring Street) and Lord Street, Curb Ramps servicing SR 2034 at the northwest, northeast, southeast and southwest corners (four State Curb Ramps).
- SR 0027 (State Street) and Grove Street, Curb Ramps servicing SR 0027 at the northwest and northeast corners (two State Curb Ramps).
- SR 0886 (North Main Street) and the Lords Gate (Alleghany College Crossing), Curb Ramps servicing SR 0886 at referenced crossing Left and Right of Centerline (two State Curb Ramps)

In the City of Erie, the following non-compliant curb ramp locations will be removed and replaced:

- SR 4016 (38th Street) and Ellsworth Street, Curb Ramp servicing Ellsworth Street (Local Ramp) at the northeast corner (one Local Curb Ramp).
- SR 4017 (Pittsburgh Avenue) and 32nd Street, Curb Ramps servicing SR 4017 at the northwest and southeast corners (two State Curb Ramps).

**D. Technically Infeasible Forms submitted by PennDOT in 2012.** PennDOT has withdrawn all TIF requests submitted in 2012. Going forward, in the event that PennDOT believes that a TIF is necessary for any curb ramp listed in Attachment A and in Attachment B, PennDOT will submit a new TIF request in accordance with the TIF provisions set forth in the prior settlement agreements between the Parties.

**E. Progress Report.** No later than August 15, 2013, PennDOT shall provide to Plaintiffs' Counsel via email a written progress report identifying PennDOT's best estimates of the progress of curb ramp construction for the curb ramps identified in Attachments A and B.

**F. "Traffic Warrant" Intersections.** At several intersections inside the Cities of Erie and Meadville, PennDOT has proposed to leave curbs in place at certain corners. These intersections in the City of Erie are listed on Attachment A at Lines 1, 3, 4, 5, 6, 8, 10, 11, 17, 20, 93, 143, 153, 158, 164, 170, 171, 173, 174; and in the City of Meadville on Attachment B at Line 1. PennDOT asserts that this is necessary due to existing "traffic warrants" for the affected intersections but Plaintiffs disagree. At these intersections, and solely in consideration for this particular Settlement Order, Plaintiffs agree that PennDOT may proceed as set forth on the relevant lines on Attachments A and B.

**G. Continuing Discussions.** The Parties have met to discuss various issues and concerns that have arisen in the course of this litigation, including but not limited to the following items:

1. **Pedestrian Access Route (PAR) Intersections:** These intersections and corners in the City of Erie are set forth on Attachment A, at Lines 14, 15, 16, 18, 19, 23, 25, 59, 104, 105, 106, 107, 110, 111, 112, 113, 126, 127, 163, 166, 167, 168, 169, 171, 172, 173, 174;

These intersections and corners in the City of Meadville are set forth on Attachment B, at Lines 33, 45, 51, 62.

2. **“Next Paving Cycle” Intersections:** At three intersections inside the City of Erie—State Route 97 (Old French Rd.) at its intersections with Cypress St., E. 33<sup>rd</sup> St. and E. 31<sup>st</sup> St. PennDOT is proposing to take until the next paving cycle to install these curb ramps. These intersections and corners are set forth on Attachment A, at Lines 96, 100, 102;
3. **Two “Proximity” Intersections:** At two intersections inside Erie, PennDOT asserts that their proximity to another intersection rules out installation of some curb ramps. Plaintiffs disagree. These intersections are Peach Street (SR 19) at Walker Street East and 38<sup>th</sup> Street (SR 4016) at Wayne Street. These intersections and corners are set forth on Attachment A, at Lines 43, 139;
4. **Running Slopes Exceeding Five Percent (5%):** The parties will continue to meet and address issues related to PennDOT construction of sidewalk segments with a running slope that exceeds 5%.

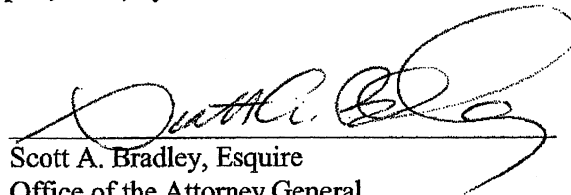
The Parties shall continue to meet as necessary to address and resolve these and any other conflicts or disagreements that may arise. If, after meeting in good faith, the parties are unable to resolve their conflicts of disagreements, any party may file a motion with the Court concerning such issue(s).

**H. Attorneys Fees.** The parties have met and have agreed upon the amount to be paid to Plaintiffs’ attorneys. This amount shall be paid *within sixty days* of the date this agreement is filed. The parties agree that this payment satisfies any and all claims for fees and costs in this lawsuit up to and through April 5, 2013 [date on Invoices]. By paying this amount, no party waives any claims or objections to claims for fees and costs that plaintiffs may seek for future actions in this lawsuit.

**I. Prior Settlement Agreements not Affected.** Except to the extent expressly set forth herein, this settlement agreement shall not affect nor alter any of the obligations, duties and/or requirements arising under or created by the settlement agreements or consent decrees which previously have been entered in this case. To the contrary, to the extent the terms and conditions of those prior settlement agreements are applicable, they are hereby incorporated by reference.

**J. Retention of Jurisdiction.** The parties expressly agree, and the Court expressly finds that the Court shall retain jurisdiction concerning the interpretation of and compliance with this Settlement Agreement.

Read and Approved on this 18<sup>th</sup> day of April, 2013, by:



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Attorneys for Commonwealth of Pennsylvania,  
Department of Transportation

**Read and Approved on this 18<sup>th</sup> day of April, 2013, by:**

ELDERKIN LAW FIRM

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Erie, Pennsylvania 16501  
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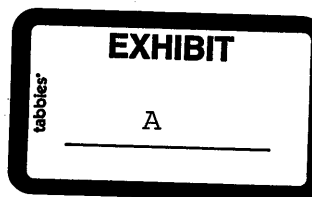
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City of Erie  
Curb Ramp Inventory  
\*  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
S. McMasters  
P.A. Miller

Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
1	ERIE	5	0501/0000	LINCOLN AVE							Y	2	2	SE and NE SW and SE	Signal Warrant. Equal access for all users is provided on 2 of the 4 legs of the signalized intersection. Prohibited crossings to remain on the west and north legs in accordance with the signal permit. No pedestrian access route exists on the northwest quadrant.
2	ERIE	5	0530/0000	CRANBERRY ST				Y	3	4				SW and NW / SE and NE NW and NE / SW	1 existing compliant ramp
3	ERIE	5	0530/0721	RASPBERRY ST							Y	0	0		Signal Warrant. Prohibited crossing to remain on SR 5 at one leg of each intersection in accordance with the signal permit. Equal access for all users is provided on 3 of the 4 legs of each signalized intersection. Existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible without impacting the existing ramps placed in 2007 with the 5-11M project. Missing pedestrian pole at southwest quadrant to be replaced at Raspberry. Based on prior correspondence between VFI and the Department regarding diagonal ramps, the existing diagonal ramps can remain but pedestrian push buttons needed upgraded to the maximum extent feasible. Also since PennDOT's policy regarding diagonals has evolved since this project was constructed, upgrading diagonal ramps will be considered with future projects that trigger the ramps and/or with projects that impact pedestrian access routes/movements. Along with diagonal ramp upgrades, prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit. Field viewed on June 6, 2012 with VFI.
4	ERIE	5	0530/1443	CASCADE ST							Y	0	0		
5	ERIE	5	0530/2166	PLUM ST							Y	0	0		
6	ERIE	5	0540/0000	LIBERTY ST							Y	0	0		
7	ERIE	5	0550/0000	POPLAR ST				Y	2	4				SW and NW / SE and NE NW / SE	2 existing compliant ramps
8	ERIE	5	0550/0720	CHERRY ST							Y	0	0		Signal Warrant. Prohibited crossing to remain on SR 5 at one leg of each intersection in accordance with the signal permit. Equal access for all users is provided on 3 of the 4 legs of each signalized intersection. Existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible without impacting the existing ramps placed in 2007 with the 5-11M project. Based on prior correspondence between VFI and the Department regarding diagonal ramps, the existing diagonal ramps can remain but pedestrian push buttons needed upgraded to the maximum extent feasible. Also since PennDOT's policy regarding diagonals has evolved since this project was constructed, upgrading diagonal ramps will be considered with future projects that trigger the ramps and/or with projects that impact pedestrian access routes/movements. Along with diagonal ramp upgrades, prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit. Field viewed on June 6, 2012 with VFI.
9	ERIE	5	0550/1442	WALNUT ST				Y	1	2				SW / SE SW	2 existing diagonals on north side, 1 existing compliant ramp on SE quadrant. 1 new diagonal on SW quadrant added since inventory was completed. Qty included to replace it.
10	ERIE	5	0560/0000	CHESNUT ST							Y	0	0		Signal Warrant. Prohibited crossing to remain on SR 5 at one leg of each intersection in accordance with the signal permit. Equal access for all users is provided on 3 of the 4 legs of each signalized intersection. Existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible without impacting the existing ramps placed in 2007 with the 5-11M project. Based on prior correspondence between VFI and the Department regarding diagonal ramps, the existing diagonal ramps can remain but pedestrian push buttons needed upgraded to the maximum extent feasible. Also since PennDOT's policy regarding diagonals has evolved since this project was constructed, upgrading diagonal ramps will be considered with future projects that trigger the ramps and/or with projects that impact pedestrian access routes/movements. Along with diagonal ramp upgrades, prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
11	ERIE	5	0560/0721	MYRTLE ST							Y	0	0		
12	ERIE	5	0621/1751	BREWSTER	Y	2	4							SW and NW / SE and NE NW and NE	
13	ERIE	5	0631/0000	PENNSYLVANIA	Y	2	4							SW and NW / SE and NE NW and NE	
14	ERIE	5	0631/0706	BRANDES AVE	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossings exist.
15	ERIE	5	0631/1015	HESS AVE	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossings exist.
16	ERIE	5	0631/1718	PAYNE AVE	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossings exist.
17	ERIE	5	0640/1962	DOWNING AVE							Y	3	3	SW and NW / NE NW and NE /SW	Signal Warrant. Equal access for all users is provided all legs of the signalized intersection. No Pedestrian access route on southeast quadrant (other than shoulders/roadway), therefore no prohibited crossing exists. Associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
18	ERIE	5	0651/0000	BACON ST	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossings exist.
19	ERIE	5	0651/0964	SENECA ST	Y	0	0								Pedestrian Access Route exists only on the north side of SR 5. No pedestrian access route on the south side of SR 5, therefore no prohibited crossings. 2 existing local ramps.





City of Erie  
Curb Ramp Inventory  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
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Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Y or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Y or No)	# of Local Ramps	# of State Ramps	Signalized (Y or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
20	ERIE	5	0660/0000	FRANKLIN AVE							Y	2		SW and SE	Signalized Tee intersection, Signal Warrant. Equal access for all users to be provided on 2 of the 3 legs of the signalized intersection. Barrier to remain on the north leg of SR 5 in accordance with signal permit. Adding a crossing for 1 leg will require partial signal upgrade (lacking junction boxes and access to poles through existing foundations) to accommodate additional pedestrian heads and push buttons. Existing ramps for SR 5 (west leg) are accessible. Proposed local crossing for Franklin Avenue. Existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
21	ERIE	5	0661/0366	GLADSTONE	Y		4							SW and SE / NW and NE	2 existing compliant ramps. Portions of sidewalk removed by others.
22	ERIE	5	0661/0979	TENTH ST	Y		2							SW and SE	2 existing compliant ramps
23	ERIE	5	0661/1874	SEVENTH ST	Y	2	0							SW and NW	No PAR. Pedestrian Access Route exists only on the west side of SR 5. No pedestrian access route on the east side of SR 5, therefore no prohibited crossing. 2 existing local ramps.
24	ERIE	8	0620/0459	BURTON AVE	Y		2							SW and NW	3 existing compliant ramps
25	ERIE	8	0620/2398	EAST AVE	Y		4-0							NW	4 existing compliant ramps. No Pedestrian Access Route on the southwest quadrant. State crossing exists on at the east side of East Ave and local crossing exists for East Ave.
26	ERIE	8	0630/0354	PERRY ST	Y		2							SW and NW	3 existing compliant ramps
27	ERIE	8	0640/0890	36TH ST		1								NW	
28	ERIE	8	0640/1341	35TH ST	Y		0		Y	4				SW and SE / NW and NE	Driveway. 4 existing compliant ramps. Existing driveway on the north side is a barrier for crossing SR 8. Crossing exists on the south side of SR 8. Local crossing exists for 35th Street. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
29	ERIE	8	0650/0498	32ND ST	Y		2							SW and NW	4 existing compliant ramps
30	ERIE	8	0650/1142	HOLMES ST	Y		2							SE and NE	4 existing compliant ramps (1 local to be replaced)
31	ERIE	8	0650/1469	MARVIN	Y	1	2							SE	
32	ERIE	19	0370/0413	MILLER AVE							Y	4	4	SW and NW / SE and NE	4 existing compliant ramps
33	ERIE	19	0371/0000	WEST GRANDVIEW (SR 4014 and TR 657)				Y		3				NW and NE / NW	9 total ramps proposed, 5 of the 9 are located in Millcreek Twp.
34	ERIE	19	0371/0839	SHERIDAN AVE	Y		4		1					NE	
35	ERIE	19	0370/1430	WEST GATE DR	Y	2	4							SW and SE / NW and NE	
36	ERIE	19	0381/0000	HILLCREST AVE	Y	2	4							SE and NE	
37	ERIE	19	0381/0000	HARDING	Y	2	2							SW and SE	Due to proximity of Harding and Mead Ave, 8 total ramps proposed. See 19-A08 plan sheet 56 of 61.
38	ERIE	19	0381/0051	MEAD AVE	Y	2	2							SW and NW	Due to proximity of Harding and Mead Ave, 8 total ramps proposed. See 19-A08 plan sheet 56 of 61.
39	ERIE	19	0380/0411	MERLINE	Y	2	4							NW and NE	
40	ERIE	19	0381/0696	AVERLON AVE	Y	2	4							SW and SE / NW and NE	
41	ERIE	19	0380/1154	39TH ST							Y	2		SW and NW	
42	ERIE	19	0381/1042	LIBERTY ST							Y	4	4	NW and NE	Due to proximity of 39th and Liberty, 10 total ramps proposed. See 19-A08 plan sheet 58 of 61.
43	ERIE	19	0381/1778	WALKER	Y	2								SW and SE / NW and NE	Due to proximity of 39th and Liberty, 10 total ramps proposed. See 19-A08 plan sheet 58 of 61.
44	ERIE	19	0391/0000	38TH (SR 4016)							Y	4	4	SW and SE / NW and NE	Safety Issue. Due to proximity of W 38th and Walker, 10 total ramps proposed. A signalized intersection (Peach Street, SR 19 and 38th Street, SR 4016) is located just north of Walker Blvd. Vehicles stopping at the light for 38th Street stack up to and adjacent to Walker Blvd causing a potentially unsafe pedestrian crossing of SR 19. Due to the proximity of the SR 4016 intersection, no crossing of SR 19 at Walker Blvd. is proposed. See Pedestrian Accommodation at Intersection Checklist dated February 1, 2013, approved February 11, 2013. See 19-A08 plan sheet 59 of 61.
45	ERIE	19	0391/0424	MAPLE AVE	Y	2	4							SW and NW / SE and NE	

City of Erie  
Curb Ramp Inventory  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
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Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
46	ERIE	19	0390/0662	CHERRY ST							Y	3	3	SW / NW and NE SW and NW / NE	2 existing compliant ramps
47	ERIE	19	0391/1112	SHUNPIKE AVE				Y	4	4				SW and SE / NW and NE SW and NW / SE and NE	
48	ERIE	19	0391/1637	RICHLEY AVE (and 34th Street)				Y	4	4				SW and SE / NW and NE SW and NW / SE and NE	
49	ERIE	19	0390/2118	MARSH ST	Y		4							SW and NW / SE and NE SW and SE / NW and NE	
50	ERIE	19	0400/0311	METZ AVE	Y	2	4							SE and NE SW and SE / NW and NE	
51	ERIE	19	0400/0354	NORMAN WAY	Y	2	4							SE and NE SW and SE / NW and NE	Added local ramp NE quadrant.
52	ERIE	19	0411/0000	31ST ST	Y	4	4							SE and NE SW and SE / NW and NE	Due to proximity of Myrtle and 31st Street, 10 total ramps proposed. See 19-ADA plan sheet 14 of 16.
53	ERIE	19	0411/0000	MYRTLE ST	Y	2	2							SW and NW NW and NE	Due to proximity of Myrtle and 31st Street, 10 total ramps proposed. See 19-ADA plan sheet 14 of 16.
54	ERIE	19	0410/0293	ORCHARD ST	Y	2	4							SW and NW SW and SE / NW and NE	
55	ERIE	19	0410/0393	MOOREHEAD							Y		1	SE	Moorehead and 29th Street consists of a a signalized plus intersection with 6 existing compliant ramps.
56	ERIE	19	0410/0393	29TH ST							Y		1	SW	Moorehead and 29th Street consists of a a signalized plus intersection with 6 existing compliant ramps.
57	ERIE	19	0410/0823	GOODRICH	Y		2	4						SW and SE/NW and NE SW and NW	Added 2 state ramps per 12/19/12 field view.
58	ERIE	19	0410/1327	HILL AVE	Y	2	4							SW and SE / NW and NE SE and NE	
59	ERIE	20	0530/0344	HAMPTON				Y		2				SW / SE	No PAR. See Photo. No Pedestrian Access Route exists on the north side of SR 20 (other than the shoulder/roadway), therefore no prohibited crossing.
60	ERIE	20	0530/1247	ELLSWORTH	Y		4		2					SW and SE SW and NW / SE and NE	
61	ERIE	20	0531/1437	BERST	Y	2	4							SW and SE SW and NW / SE and NE	
62	ERIE	20	0530/1956	HARVARD	Y	2	4							SW and SE SW and NW / SE and NE	1 existing compliant ramp.
63	ERIE	20	0531/2156	HAYBARGER	Y	1	4							SW SW and NW / SE and NE	
64	ERIE	20	0531/2258	CHARLOTTE	Y	2	2							NW and NE SE and NE	Driveway. 2 existing compliant ramps. Existing driveway on the west side is a barrier for crossing SR 20. Proposed crossing on the east side of SR 20. Local crossing exists for Charlotte Street. Field viewed on December 19,2012 with VFI; proposed ramp locations acceptable.
65	ERIE	20	0530/2551	OAKWOOD	Y		4							SW and NW / SE and NE SW and SE	
66	ERIE	20	0540/0402	AUBURN	Y	2	4							SW and NW / SE and NE SW and SE	
67	ERIE	20	0540/0856	EMERSON	Y	2	4							SW and NW / SE and NE SW and SE	
68	ERIE	20	0540/1156	WASHINGTON	Y	2	4							SW and NW / SE and NE SW and SE	
69	ERIE	20	0541/1422	BAUR	Y	2	4							SW and NW / SE and NE NW and NE	
70	ERIE	20	0540/1563	PEBBLE	Y	2	4							SW and NW / SE and NE SW and SE	
71	ERIE	20	0540/1830	MELROSE	Y	2	4							SW and NW / SE and NE SW and SE	
72	ERIE	20	0550/0307	ELMWOOD SOUTH	Y		4							SW and NW / SE and NE	2 existing compliant ramps.
73	ERIE	20	0550/0643	CRAWBERRY ST	Y		4							SW and NW / SE and NE	2 existing compliant ramps.
74	ERIE	20	0560/0726	POPLAR SOUTH	Y		4							SW and NW / SE and NE	
						2								SW and SE	

City of Erie  
Curb Ramp Inventory  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
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Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
75	ERIE	20	0560/0726	POPLAR NORTH	Y		4							SW and NW / SE and NE	2 existing compliant ramps
76	ERIE	20	0560/1040	HAZEL	Y	2								SE and NE SW and SE	Driveway. Existing driveway on the west side is a barrier for crossing SR 20. Proposed crossing on the east side of SR 20. Proposed local crossing for Hazel Street. Field viewed on December 19, 2012 with VFI; proposed ramp locations acceptable.
77	ERIE	20	0560/1347	MAPLE ST	Y	2	4							SW and NW / SE and NE SW and SE	
78	ERIE	20	0560/1904	SIGSBEE	Y	2	4							SW and NW / SE and NE SW and SE	
79	ERIE	20	0560/2624	SCHELY	Y	2	4							SW and NW / SE and NE SW and SE	
80	ERIE	20	0570/0359	COCHRAN	Y	1	2							SW and NW SW	3 existing compliant ramps.
81	ERIE	20	0570/0988	GOODRICH	Y	1	2							SE and NE	4 existing compliant ramps.
82	ERIE	20	0610/1131	MONROE	Y	1	2							SE and NE SE	3 existing compliant ramps.
83	ERIE	20	0610/1583	JACKSON	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the east side is a barrier for crossing SR 20. Crossing exists on the west side of SR 20. Local crossing exists for Jackson Street. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
84	ERIE	20	0610/1816	VAN BUREN	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the west side is a barrier for crossing SR 20. Crossing exists on the east side of SR 20. Local crossing exists for Van Buren Street. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
85	ERIE	20	0640/0320	KILPATRICK	Y	2	4							SW and NW / NE and SE SW and SE	Added 2 state ramps per 12/19/12 field view.
86	ERIE	20	0640/1227	SCHAALL	Y	2	4							SW and NW / SE and NE NW and NE	
87	ERIE	20	0650/0000	CAMERON	Y	2	4							SW and NW / SE and NE SW and SE	
88	ERIE	20	0650/0131	THOMPSON	Y	2	2							SW / SE	4 existing compliant ramps.
89	ERIE	20	0650/0339	BROOKLYN	Y	2	2							SW and NW NW and NE	Driveway. Existing driveway on the eastside is a barrier for crossing SR 20. Proposed crossing on the west side of SR 20. Proposed local crossing for Brooklyn Ave. Field viewed on December 19, 2012 with VFI; proposed ramp locations acceptable.
90	ERIE	20	0650/0779	N. McCAIN	Y	2	4							SW and NW / SE and NE NW and NE	
91	ERIE	20	0650/0860	S. McCAIN	Y	2	4							SW and NW / SE and NE SW and SE	
92	ERIE	20	0650/1566	JUNE ST				Y		4	4			SW and NW / SE and NE SW and SE / NW and NE	
93	ERIE	20	0650/1948	FRANKLIN AVE							Y	0			Signal Warrant. Equal access for all users to be provided on 2 of the 3 legs of the signalized intersection. Barrier to remain on the east leg of SR 20 in accordance with signal permit. Adding a crossing for 1 leg will require a signal upgrade to accommodate additional pedestrian heads and push buttons. Existing ramps for SR 20 (west leg) are accessible. Proposed local ramp for Franklin Avenue (northeast quadrant). Existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
94	ERIE	20	0660/0756	HARRISON AVE	Y	2	4							SW and NW / SE and NE SW and SE	
95	ERIE	97 (197)	0420/1040	42ND ST	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the north side is a barrier for crossing SR 97. Crossing exists on the south side of SR 97. Local crossing exists for 42nd Street. SR 97 (197)-01M project constructed in 2011. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
96	ERIE	97 (197)	0420/1215	CYPRESS ST	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the north side is a barrier for crossing SR 97. Crossing exists on the south side of SR 97. Local crossing exists for Cypress Street. SR 97 (197)-01M project constructed in 2011. Field viewed on December 19, 2012 with VFI; propose 2 additional ramps for next paving cycle.
97	ERIE	97 (197)	0420/2152	39TH ST	Y	2								SW and SE	4 existing compliant ramps
98	ERIE	97 (197)	0440/0000	MAIDEN LANE	Y	2								SW and SE	4 existing compliant ramps
99	ERIE	97 (197)	0440/0130	33RD ST	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the south side is a barrier for crossing SR 97. Crossing exists on the north side of SR 97. Local crossing exists for 33rd Street. SR 97 (197)-01M project constructed in 2011. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
100	ERIE	97 (197)	0440/0295	33RD ST	Y	0									Driveway. 4 existing compliant ramps. Existing driveway on the south side is a barrier for crossing SR 97. Crossing exists on the north side of SR 97. Local crossing exists for 33rd Street. SR 97 (197)-01M project constructed in 2011. Field viewed on December 19, 2012 with VFI; propose 2 additional ramps for next paving cycle.
101	ERIE	97 (197)	0440/0622	32ND ST	Y	2								NW and NE	4 existing compliant ramps

City of Erie  
Curb Ramp Inventory  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
S. McMasters  
P.A. Miller

Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
102	ERIE	97 (197)	0440/1003	31ST ST	Y	0	0								Driveway. 4 existing compliant ramps. Existing driveway on the north side is a barrier for crossing SR 97. Crossing exists on the south side of SR 97. Local crossing exists for 31st Street. SR 97 (197)-01M project constructed in 2011. Field viewed on December 19, 2012 with VFI; propose 2 additional ramps for next paving cycle.
103	ERIE	97 (197)	0440/1738	29TH ST	Y	0	0								Sight Distance. 4 existing compliant ramps. Prohibited crossing on the north side due to inadequate sight distance (existing sight distance is 135', required sight distance is 140'). Existing crossing on the south side. Existing local crossing for 29th St. Field viewed on December 19, 2012 with VFI; current ramp locations acceptable.
104	ERIE	505	0060/0427	MARSH ST	Y	0	0								No PAR on east side of 505. 2 existing local ramps. Pedestrian Access Route exists only on the west side of SR 505. No pedestrian access route on the east side of SR 505 (guiderail), therefore no prohibited crossing.
105	ERIE	505	0060/0913	CRAIG ST	Y	0	0								No PAR on east side of 505. 2 existing local ramps. Pedestrian Access Route exists only on the west side of SR 505. No pedestrian access route on the east side of SR 505 (guiderail), therefore no prohibited crossing.
106	ERIE	505	0060/1233	METZ AVE	Y	0	0								No PAR on east side of 505. 2 existing local ramps. Pedestrian Access Route exists only on the west side of SR 505. No pedestrian access route on the east side of SR 505 (guiderail), therefore no prohibited crossing.
107	ERIE	505	0060/1487	NORMAN WAY				Y		2				SW and SE	1 existing compliant ramp NW quadrant. No PAR NW to NE and NE to SE (No PAR in northeast quadrant). Ramp on SE quadrant to be built with Norman Way Bridge Project in 2013. (1S and 1L constructed in 2012 with 505-211 project.) See Norman Way plan sheet 12 of 13
108	ERIE	505	0070/0324	29TH ST	Y	2	4		1					SW SW and SE / NW and NE SE and NE	
109	ERIE	505	0070/0733	HILL RD	Y	2	4							SW and SE / NW and NE SW and NW	
110	ERIE	1001	0270/1469	E GRANDVIEW				Y		0					No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossing. Field viewed on December 19, 2012 with VFI; VFI proposed level landings with DWS. PennDOT does not agree.
111	ERIE	1001	0270/2134	E 43RD ST	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossing. Field viewed on December 19, 2012 with VFI; VFI proposed level landings with DWS. PennDOT does not agree.
112	ERIE	1001	0270/2446	E 42ND ST	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossing. Field viewed on December 19, 2012 with VFI; VFI proposed level landings with DWS. PennDOT does not agree.
113	ERIE	1001	0270/2738	E 41ST ST	Y	0	0								No PAR. No Pedestrian Access Route exists (other than the shoulder/roadway), therefore no prohibited crossing. Field viewed on December 19, 2012 with VFI; VFI proposed level landings with DWS. PennDOT does not agree.
114	ERIE	4016	0140/0872	SCHAPER AVE				Y		4				SW and NW / SE and NE SW and SE / NW and NE	2 new local ramps constructed adjacent to existing sidewalk (may need reconstructed).
115	ERIE	4016	0141/1398	STOUGH AVE	Y	2	4							SW and NW / SE and NE NW and NE	
116	ERIE	4016	0140/2087	GARDEN AVE				Y		4				SW and NW / SE and NE SW and SE / NW and NE	
117	ERIE	4016	0150/0000	ELLSWORTH ST				Y		2				SW and NW SW	5 existing compliant ramps.
118	ERIE	4016	0150/0374	HARVARD AVE	Y	2	4		1					SW and NW / SE and NE NW and NE	
119	ERIE	4016	0150/1050	CHARLOTTE	Y	2	4							SW and NW / SE and NE NW and NE	
120	ERIE	4016	0150/1338	OAKWOOD	Y	2	4							SW and NW / SE and NE NW and NE	
121	ERIE	4016	0150/2940	WASHINGTON AVE	Y	2	4							SW and NW / SE and NE NW and NE	
122	ERIE	4016	0170/0719	CASCADE ST	Y	2	4							SW and NW / SE and NE NW and NE	City completed 2 locals, may need reconstructed.
123	ERIE	4016	0170/0886	AVERLON	Y		2								No crossing on west side due to proximity of Cascade Street. Prohibited crossing to remain on the West side of Averlon Avenue due to the skew angle and proximity of Averlon Avenue with Cascade Street (6 ramps proposed for Cascade Street, Tee intersection). Field viewed on December 19, 2012 with VFI; 2 additional ramps added crossing SR 4016 on the east side of Averlon Ave. Existing compliant local ramps.
124	ERIE	4016	0171/1447	PLUM ST	Y	2	2							SE and NE NW and NE	Driveway. Existing Driveway on the west side is a barrier for crossing SR 4016. Proposed crossing on the east side of SR 4016. Proposed local crossing for Plum Street. Field viewed on December 12, 2012 with VFI, proposed ramp locations acceptable.
125	ERIE	4016	0171/2182	POPLAR	Y	2	4							SW and NW / SE and NE NW and NE	
126	ERIE	4016	0190/0000	MYRTLE ST	Y		2							NW / NE	No PAR SW and SE quadrants. Pedestrian Access Route exists only on the north side of SR 4016. No pedestrian access route on the south side of SR 4016 (other than roadway/shoulders), therefore no prohibited crossing. See Photo. (Ramps constructed)
127	ERIE	4016	0190/0271	ELIOT ROAD				Y		0					4 existing compliant ramps. No Pedestrian Access Route southwest quadrant (other than roadway/shoulders), therefore no prohibited crossing. State

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Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
128	ERIE	4016	0190/0385	SASSAFRAS ST				Y	0	2				SE and NE NE	crossing on the east side of SR 4016 and local crossing on the north side for Eliot Road. See Photo. 5 existing compliant ramps.
129	ERIE	4016	0190/0654	TRASK AVE	Y	4			1					SW and NW / SE and NE SW and SE	
130	ERIE	4016	0200/0410	FRENCH	Y	3									Tee intersection with VA Hospital Driveway. 3 existing compliant ramps. Median on east side.
131	ERIE	4016	0200/1140	HOLLAND ST	Y	2								SW and NW NW	3 existing compliant ramps.
132	ERIE	4016	0210/0260	FRUIT ST	Y	4								SW and NW / SE and NE SW and SE	
133	ERIE	4016	0210/0603	SUNSET	Y	4								SW and NW / SE and NE SW and SE	
134	ERIE	4016	0210/1001	PARADE ST				Y	4	4				SW and NW / SE and NE SW and SE / NW and NE SW and NW / SE and NE NW and NE	City completed 2 locals, may need reconstructed.
135	ERIE	4016	0221/0669	ASH ST	Y	4								SW and NW / SE and NE SW and NW / SE and NE NW and NE	City completed 2 locals, may need reconstructed.
136	ERIE	4016	0220/1127	BRIGGS AVE	Y	4								SW and NW / SE and NE SW and SE	City completed 2 locals, may need reconstructed.
137	ERIE	4016	0221/1357	REED ST	Y	4								SW and NW / SE and NE NW and NE	
138	ERIE	4016	0220/1576	LEWIS AVE	Y	6								SW and NW / SE and NE / Median SW and SE	2 additional ramps for median.
139	ERIE	4016	0231/0125	WAYNE ST	Y	0									Safety Issue. 2 existing local ramps. A signalized intersection (Pine and SR 4016) is located just west of Wayne street. Vehicles stopping at the light for Pine street will stack up to and adjacent to Wayne Street causing a potentially unsafe pedestrian crossing of SR 4016. Due to the proximity of Pine Street, no crossing of SR 4016 at Wayne Street is proposed. See Pedestrian Accommodation at Intersection Checklist dated December 21, 2012, approved January 9, 2013. Current local ramps for Wayne Street should be updated by the city. Field viewed on December 19, 2012 with VFI, VFI proposed state ramps at this location and PennDOT does not agree based on the results of the Pedestrian Study.
140	ERIE	4016	0231/0996	PERRY ST N.	Y	4								SW and NW / SE and NE NW and NE	
141	ERIE	4016	0231/0803	PERRY ST S.	Y	4								SW and NW / SE and NE SW and SE	
142	ERIE	4016	0230/1302	LAKE PLEASANT	Y	3	4							SW and NW / SE SW and SE	Added 1 State Ramp
143	ERIE	4016	0230/1581	EAST AVE							Y	0			Signal Warrant. Equal access for all users is provided on 3 of the 4 legs of the signalized intersection. Prohibited crossing to remain on the west side of SR 4016 in accordance with signal permit. Adding crossing for 1 leg will require complete signal upgrade to accommodate additional pedestrian heads and push buttons. Existing ramps constructed with SR 5-ADA project let in 2009. Since these are new ramps, existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit. Field viewed on June 6, 2012 with VFI.
144	ERIE	4016	0230/1880	STANLEY AVE	Y	4								SW and NW / SE and NE SW and SE	
145	ERIE	4016	0230/2176	PENNSYLVANIA				Y	4	4				SW and NW / SE and NE SW and SE / NW and NE SW and NW / SE and NE SE and SW	
146	ERIE	4016	0240/0000	ESSEX AVE	Y	2	2							SW and NW / SE and NE SW and SE / NW and NE SW	Driveway. Existing Driveway on the east side is a barrier for crossing SR 4016. Proposed crossing on the west side of SR 4016. Proposed local crossings for Essex Avenue. Field viewed on December 19, 2012 with VFI, proposed ramp locations acceptable.
147	ERIE	4016	0240/0491	BRANDES AVE				Y	4	4				SW and NW / SE and NE SW and SE / NW and NE SW	
148	ERIE	4016	0240/0814	BURTON AVE							Y	1	1	SW SW	6 existing compliant ramps.
149	ERIE	4016	0240/1134	DEXTER	Y	4								SW and NW / SE and NE SW and SE	
150	ERIE	4016	0240/1478	TUTTLE AVE				Y	4	4				SW and NW / SE and NE SW and SE / NW and NE SW and NW / SE and NE	
151	ERIE	4016	0241/1981	WARSAW AVE	Y	4								SW and NW / SE and NE	

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12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
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Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized <sup>1,2,3</sup> Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
152	ERIE	4016	0251/0000	MAXWELL AVE	Y	2	4							SW and SE SW and NW / SE and NE SW and SE	
153	ERIE	4016	0250/0316	DAVISON AVE							Y	0			Signal Warrant. Equal access provided on 3 of the 4 legs of the signalized intersection. Barrier to remain on the west side of SR 4016 in accordance with signal permit. Adding crossing for 1 leg will require complete signal upgrade to accommodate additional pedestrian heads and push buttons. Existing ramps constructed with SR 5-ADA project let in 2009. Since these are new ramps, existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit. Field viewed on June 6, 2012 with VFI.
154	ERIE	4016	0250/1225	MARION	Y	2	4							SW and NW / SE and NE SW and SE	
155	ERIE	4016	0250/1862	FARGO ST	Y	2	4							SW and NW / SE and NE SW and SE	
156	ERIE	4016	0260/0000	PAGE ST				Y	2	2				SW and NW	Driveway. Existing Driveway on the east side is a barrier for crossing SR 4016. Proposed crossing on the west side of SR 4016. Proposed local crossings for Page Street. Field viewed on December 19, 2012 with VFI; proposed ramp locations acceptable (added 2 local ramps).
157	ERIE	4016	0260/0334	CONRAD ST	Y	2	4		2-4					NW and NE / SW and SE SW and NW / SE and NE SW and SE	
158	ERIE	4016	0261/0490	ZIMMERMAN RD							Y	0			Signal Warrant. Equal access for all users is provided on 3 of the 4 legs of the signalized intersection. Prohibited crossing to remain on the east side of SR 4016 in accordance with signal permit. Adding crossing for 1 leg will require complete signal upgrade to accommodate additional pedestrian heads and push buttons. Existing ramps constructed with SR 5-ADA project let in 2009. Since these are new ramps, existing non-compliant pedestrian push buttons will be upgraded to the maximum extent feasible. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit. Field viewed on June 6, 2012 with VFI.
159	ERIE	4016	0261/0992	MCCAIN	Y	2	4							SW and NW / SE and NE NW and NE	
160	ERIE	4016	0260/1813	NANCY AVE	Y	2	4							SW and NW / SE and NE SW and SE	
161	ERIE	4016	0260/2172	CARNEY AVE	Y	2	4							SW and NW / SE and NE SW and SE	
162	ERIE	4016	0260/2519	ENGLISH AVE	Y	2	4							SW and NW / SE and NE SW and SE	
163	ERIE	4016	0280/0000	RICE AVE	Y	1	2							SW and NW / NE SW	No PAR. No Pedestrian Access Route in the northeast and southeast quadrants. Proposed crossing on the west side of SR 4016 and proposed local ramp in the southwest quadrant. See Photo.
164	ERIE	4017	0010/0000	38TH ST (SR 4016)							Y	0			Signal Warrant. Equal access for all users is provided on 3 of the 4 legs of the signalized intersection. Prohibited crossing to remain on the east side of SR 4017 in accordance with signal permit. Adding crossing for 1 leg will require complete signal upgrade to accommodate additional pedestrian heads and push buttons. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
165	ERIE	4017	0010/2062	32ND ST							Y	1			7 existing compliant ramps
166	ERIE	4017	0040/0887	22ND ST	Y	1								NE	PAR only in NE quadrant. No Pedestrian Access Route (other than roadway/shoulders), except in northeast quadrant. Access to Pedestrian Access Route provided by proposed local ramp in northeast quadrant. See Photo.
167	ERIE	4017	0040/1171	21ST ST				Y	1					SE	No PAR. 1 existing compliant ramp NE quadrant. No Pedestrian Access Route (other than roadway/shoulders) on the west side of SR 4017. Existing state and local ramps in northeast quadrant, proposed state and local ramps in south east quadrant. See Photo.
168	ERIE	4017	0060/0735	MANOR DR.	Y	2			1					SE and NE	No PAR. No Pedestrian Access Route (other than roadway/shoulders) on the west side of SR 4017. Proposed local crossing for Manor Drive. See Photo.
169	ERIE	4017	0060/1077	WEST ERIE PLAZA DRIVE	Y	2								SW and SE	Driveway and No PAR. Driveway and no Pedestrian Access Route in northeast quadrant. Propose state crossing on the south side of Manor Drive and proposed local crossing for Manor Drive. See photo.
170	ERIE	4017	0080/0000	W 8TH ST (SR 4018)		2					Y	0		SW and NW	Signal Warrant. Equal access for all users is provided on 2 of the 4 legs of the signalized intersection. Prohibited crossing to remain on the west and south legs in accordance with the signal permit. Adding crossings for the 2 legs will require complete signal upgrade to accommodate additional pedestrian heads and push buttons. Prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.

City of Erie  
Curb Ramp Inventory  
"T", Plus, and Signalized Intersections

12-14-11 REVISED: 7-26-12, 1-23-13, 2-4-13  
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P.A. Miller

Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Year or No)	# of Local Ramps	# of State Ramps	Signalized (Year or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	Comments
171	ERIE	4034	0012/0000	LINCOLN AVE							Y		1	SE	Signal Warrant. No PAR SW quadrant. Existing diagonal NE Quadrant. Existing compliant Ramp NW quadrant. Equal access for all users is provided on 2 of the 4 legs of the signalized intersection. Prohibited crossings to remain on the west side of SR 4034 and on the south side of Lincoln Street in accordance with the signal permit. No pedestrian access route exists on the southwest quadrant. Also since PennDOT's policy regarding diagonals has evolved since this project was constructed, upgrading diagonal ramps will be considered with future projects that trigger the ramps and/or with projects that impact pedestrian access routes/movements. Along with diagonal ramp upgrades, prohibited legs and associated accessibility features (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements, thus requiring a new signal permit.
172	ERIE	4034	0040/0015	CRANBERRY ST	Y	0	0								No PAR. Existing crossing provided on the south side of SR 4034. No pedestrian access route exists on the northeast quadrant. Field viewed on June 6, 2012 with VFI.
173	ERIE	4034	0070/1140	HOLLAND ST							Y	0	0		Signal Warrant. Equal access for all users is provided on 2 of the 4 legs of the signalized intersection. Prohibited crossings to remain on the west side of SR 4034 and for Holland Street in accordance with the signal permit. No pedestrian access route exists on the southwest quadrant. Existing railroad crossing of Holland Street at this intersection. Field viewed on June 6, 2012 with VFI.
174	ERIE	4034	0085/0000	PORT ACCESS							Y	0	0		Signal Warrant. Equal access for all users provided on 2 of the 4 legs of the signalized intersection. Prohibited crossings to remain on the west and south side of SR 4034 in accordance with the signal permit. Guiderail and no pedestrian access route on the south west quadrant.
						160	323		53	64		22	28		
				Total ramps to be constructed				650							
															Intersections reviewed by: Rod Fasenmyer, PennDOT Project Manager Scott McMasters, PennDOT Highway Designer Bob Zarecky, PennDOT ADA Coordinator Paul A. Miller, P.E., PennDOT Plans Development Engineer
															Locations noted reviewed by the following on June 6, 2012: Alan Dunfee, VFI (available for the morning portion of the field view) Rick Hoffman, VFI Michael Simko, VFI Rod Fasenmyer, PennDOT Project Manager Scott McMasters, PennDOT Highway Designer Bob Zarecky, PennDOT ADA Coordinator Paul A. Miller, P.E., PennDOT Plans Development Engineer
															Locations noted reviewed by the following on December 19, 2012: J. Mark Finnigan, Esquire Craig A. Markham, Esquire Rick Hoffman, VFI Scott A. Bradley, Senior Deputy Attorney General Bob Zarecky, PennDOT ADA Coordinator Paul A. Miller, P.E., PennDOT Plans Development Engineer

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Permit No. 25-002 Sheet 2 Of 2  
Date Issued 9-8-92 Date Revised 11-24-97

**GENERAL NOTES**

INSTALLATION, OPERATION AND MAINTENANCE OF THIS TRAFFIC SIGNAL SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS ON OFFICIAL TRAFFIC CONTROL DEVICES.

NO MODIFICATION OF THIS INSTALLATION IS PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE DEPARTMENT.

ALL MAINTENANCE NECESSARY FOR PROPER VISIBILITY OF THE SIGNALS, INCLUDING TRIMMING TREES, IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED BY THE PERMITTEE, UNLESS OTHERWISE INDICATED. EXCEPT THE LONGITUDINAL PAVEMENT MARKINGS ON STATE HIGHWAYS WHICH WILL BE MAINTAINED BY THE DEPARTMENT.

POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET BEHIND THE FACE OF THE CURB OR CORNER OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM HORIZONTAL CLEARANCE OF 2 FEET.

THE BOTTOM OF SIGNAL HEADS AND SIGNS ERECTED OVER THE ROADWAY SHALL BE 17 FEET ABOVE THE ROADWAY. THE BOTTOM OF POST MOUNTED SIGNAL HEADS SHALL NOT BE LESS THAN 10 FEET NOR MORE THAN 15 FEET ABOVE THE SIDEWALK OR PAVEMENT GRADE.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNAL HEADS MEASURED AT RIGHT ANGLES TO THE APPROACH SHALL BE 8 FEET.

IN ADDITION TO THIS SIGNAL PERMIT, THE PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT PRIOR TO ANY OPENINGS BEING MADE IN OR UNDER ANY PORTION OF A STATE HIGHWAY.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLES WITH THE PROVISIONS OF ACT 287. PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, PRIOR TO INSTALLATION CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE DEPARTMENT OF TRANSPORTATION PAVEMENT MARKING HANDBOOK.

**EMERGENCY VEHICLE PREEMPTION NOTE**

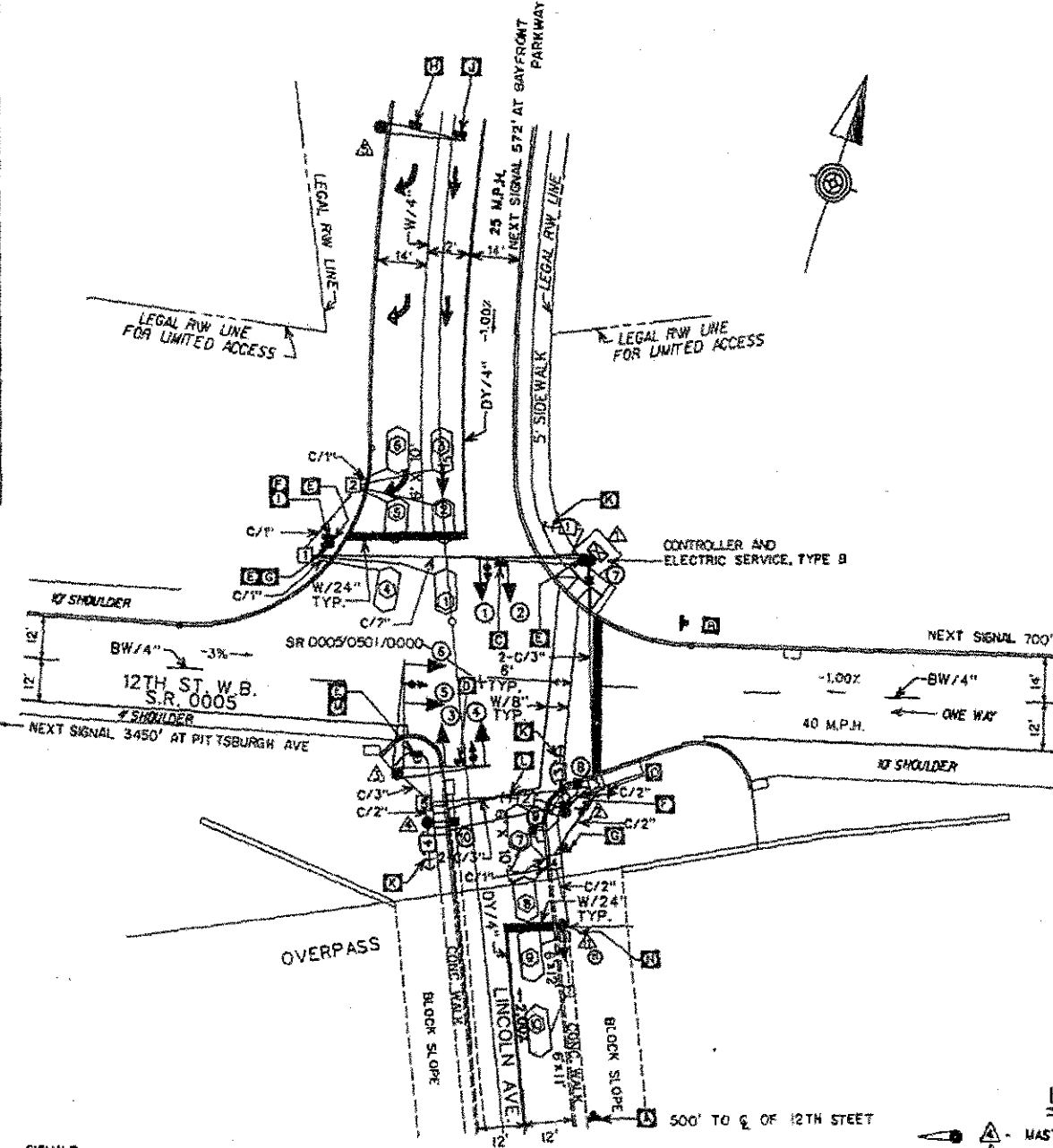
THE EMERGENCY VEHICLE PREEMPTION UNIT ACTIVATES THE INTERNAL CONTROLLER PREEMPTION SEQUENCES. UPON RECEIVING A SIGNAL FROM AN EMERGENCY VEHICLE, THE CONTROLLER WILL, IN THE "WALK" INTERVAL OF A CONFLICTING PHASE AND UTILIZING CONTROLLER INTERVAL TIMINGS, PROVIDE A FLASHING "DON'T WALK" INTERVAL FOLLOWED BY A "YELLOW" CHANGE INTERVAL AND AN "ALL RED" INTERVAL RESULTING IN THE SELECTED STREET BECOMING "GREEN" WITH ALL OTHERS REMAINING "RED". IF PREEMPTED DURING ANY OTHER INTERVAL, THE INTERVAL WILL TIME OUT AND PROCEED AS DESCRIBED ABOVE. A PRE-EMPTION INDICATION WILL BE PROVIDED TO THE SELECTED STREET WITH THE GREEN. UPON TERMINATION OF PRE-EMPTION, SIGNAL WILL RETURN TO NORMAL OPERATION.

PHASE		1				2				
1, 2, 3, 4	R	R	R	R	G	G	Y	R	R	
5, 6	G	G	Y	R	R	R	R	R	Y	
7, 8	D	D	W	D	D	D	W	D	D	
9, 10	D	D	W	D	D	D	W	D	D	
FIXED			4	2			3	2		
PASSAGE										
MAX I			60				20			
MAX II			60				20			
PEDESTRIANS	7	7			8	12				
MEMORY			MAX. RECALL				NON-LOCKING			

\* UPON PEDESTRIAN ACTUATION ONLY

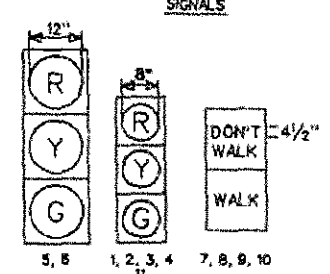
TRNG PLAN	HOURS	CYCLE LENGTH (SEC)	PHASE MAX GREEN		OFFSET (SEC)
			1	2	
2	6:30 TO 9:30	90	60	20	32
3	3:00 TO 6:00	90	61	19	33
1	9:30AM TO 3:00PM 6:00PM TO 6:30AM, SAT. & SUN.	80	52	18	20

- 1 CONTROLLER IS INTERCONNECTED WITH ADJACENT SIGNALS TO PROVIDE TRAFFIC PROGRESSION ON 12TH STREET, AND SUPERVISED BY THE MASTER CONTROLLER AT 12TH AND GREENGARDEN.
- 2 OFFSET REFERENCED TO THE END OF EB/WB GREEN AT WEST 12TH STREET AND GREENGARDEN ROAD.



**SIGN TABULATION**

PLAN SYMBOL	DESCRIPTION	SIZE W x H
A	W3-3 SIGNAL AHEAD	36" x 36"
B	R5-9 WRONG WAY	36" x 24"
C	R6-2L VERTICAL LEFT ONEWAY	30" x 36"
D	R6-2R VERTICAL RIGHT ONEWAY	30" x 36"
E	R9-3 NO PEDESTRIAN CROSSING	18" x 18"
F	R6-2R VERTICAL RIGHT ONEWAY	24" x 30"
G	R6-2L VERTICAL LEFT ONEWAY	24" x 30"
H	R3-5R RIGHT TURN	30" x 36"
I	R3-7R RIGHT LANE MUST TURN RIGHT	30" x 30"
J	R3-5S STRAIGHT THROUGH	30" x 36"
K	R10-4R PUSH BUTTON FOR WALK SIGNAL	9" x 12"
L	R10-4L PUSH BUTTON FOR WALK SIGNAL	9" x 12"
M	R9-3-R USE CROSSWALK	18" x 12"
N	R10-6L STOP HERE ON RED	24" x 30"
O	R5-1 DO NOT ENTER	30" x 30"



NOTE: ALL SIGNALS TO HAVE TUNNEL VISORS

- LEGEND**
- ▲ - MAST ARM
  - - STRAIN POLE
  - - PEDESTAL
  - ⊙ - VEHICULAR SIGNAL HEAD
  - ⊙ - PEDESTRIAN SIGNAL HEAD
  - ⊙ - SIGN
  - ⊙ - VEHICLE DETECTOR
  - ⊙ - JUNCTION BOX
  - ⊙ - PEDESTRIAN PUSH BUTTON/SIGN
  - ⊙ - CONTROLLER ASSEMBLY
  - W/4" - SOLID WHITE LINE/WIDTH
  - BW/4" - BROKEN WHITE LINE/WIDTH
  - Y/4" - SOLID YELLOW LINE/WIDTH
  - BY/4" - BROKEN YELLOW LINE/WIDTH
  - DY/4" - DOUBLE SOLID YELLOW LINE/WIDTH
  - ⊙ - PRE-EMPTION (RCP) DEVICE RADIO CONTROLLED

COUNTY **ERIE**

MUNICIPALITY **CITY OF ERIE**

INTERSECTION **LINCOLN AVE. AND S.R. 0005, 12TH STREET**

REVIEWED *[Signature]* **DATE**

RECOMMENDED *[Signature]* **DATE**

DISTRICT TRAFFIC ENGINEER

SCALE IN FT. 0 20 40 80

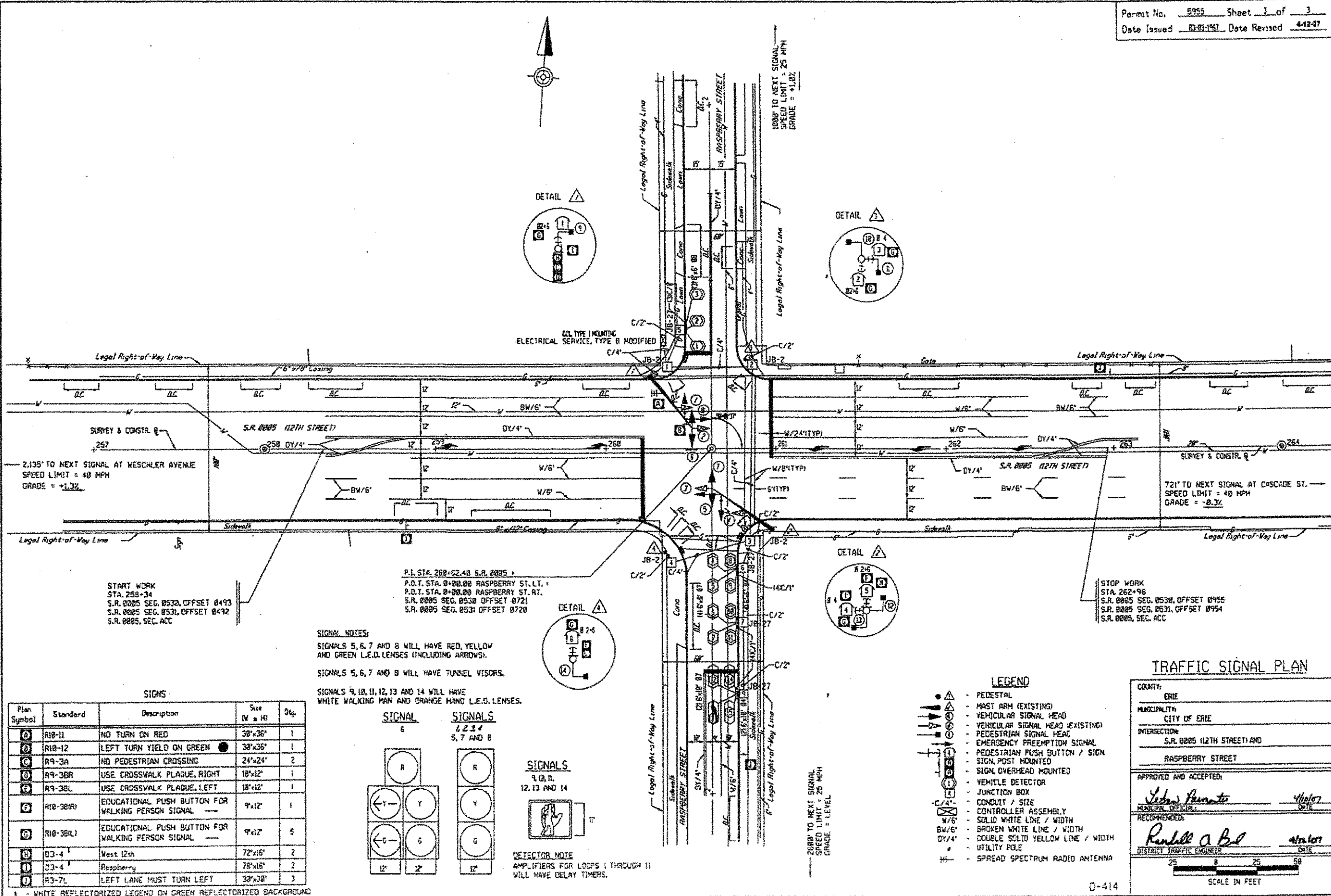
D-409



#3

Permit No. 5955 Sheet 3 of 3  
Date Issued 03-01-1961 Date Revised 4-12-47

OPERATION FILE NAME: U:\Virt\Traffic Signal\Permit Drawing\Traffic Signal\Erie\City of Erie\12th St and Raspberry St. ACC. 2120  
0-902 CUB 102-80 REVISED 1-10-00 PLOTTED 21-MAR-2007 09:24



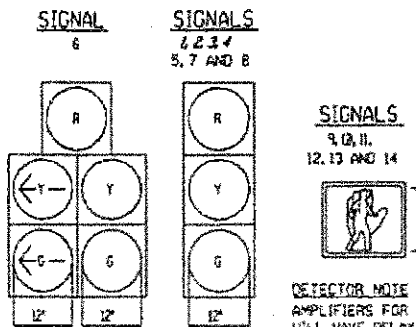
START WORK  
STA. 258+34  
S.R. 0805 SEG. 0530, OFFSET 0493  
S.R. 0805 SEG. 0531, OFFSET 0492  
S.R. 0805, SEC. ACC

P.I. STA. 260+62.40 S.R. 0805 =  
P.O.T. STA. 0+00.00 RASPBERRY ST. LT. =  
P.O.T. STA. 0+00.00 RASPBERRY ST. RT. =  
S.R. 0805 SEG. 0530 OFFSET 0721  
S.R. 0805 SEG. 0531 OFFSET 0720

STOP WORK  
STA. 262+96  
S.R. 0805 SEG. 0530, OFFSET 0955  
S.R. 0805 SEG. 0531, OFFSET 0954  
S.R. 0805, SEC. ACC

**SIGNAL NOTES:**  
SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.D. LENSES (INCLUDING ARROWS).  
SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.  
SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.

Plan Symbol	Standard	Description	Size IV x HI	Qty
1	R18-11	NO TURN ON RED	30"x36"	1
2	R18-12	LEFT TURN YIELD ON GREEN	30"x36"	1
3	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
4	R9-3BR	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
5	R9-3BL	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
6	R18-38(R)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	1
7	R18-38(L)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	5
8	D3-4	West 12th	72"x15"	2
9	D3-4	Raspberry	78"x15"	2
10	R3-7L	LEFT LANE MUST TURN LEFT	32"x30"	3



DETECTOR NOTE  
AMPLIFIERS FOR LOOPS 1 THROUGH 11  
WILL HAVE DELAY TIMERS.

- LEGEND**
- ▲ PEDESTAL
  - ▲ MAST ARM (EXISTING)
  - VEHICULAR SIGNAL HEAD
  - VEHICULAR SIGNAL HEAD (EXISTING)
  - PEDESTRIAN SIGNAL HEAD
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTACTOR ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

**TRAFFIC SIGNAL PLAN**

CITY: ERIE  
MUNICIPALITY: CITY OF ERIE  
INTERSECTION: S.R. 0805 (12TH STREET) AND RASPBERRY STREET

APPROVED AND ACCEPTED:  
*John P. ...* (Municipal Official)  
*Richard A. ...* (District Traffic Engineer)

RECOMMENDED:  
*...* (Municipal Official)  
*...* (District Traffic Engineer)

SCALE IN FEET: 25 50

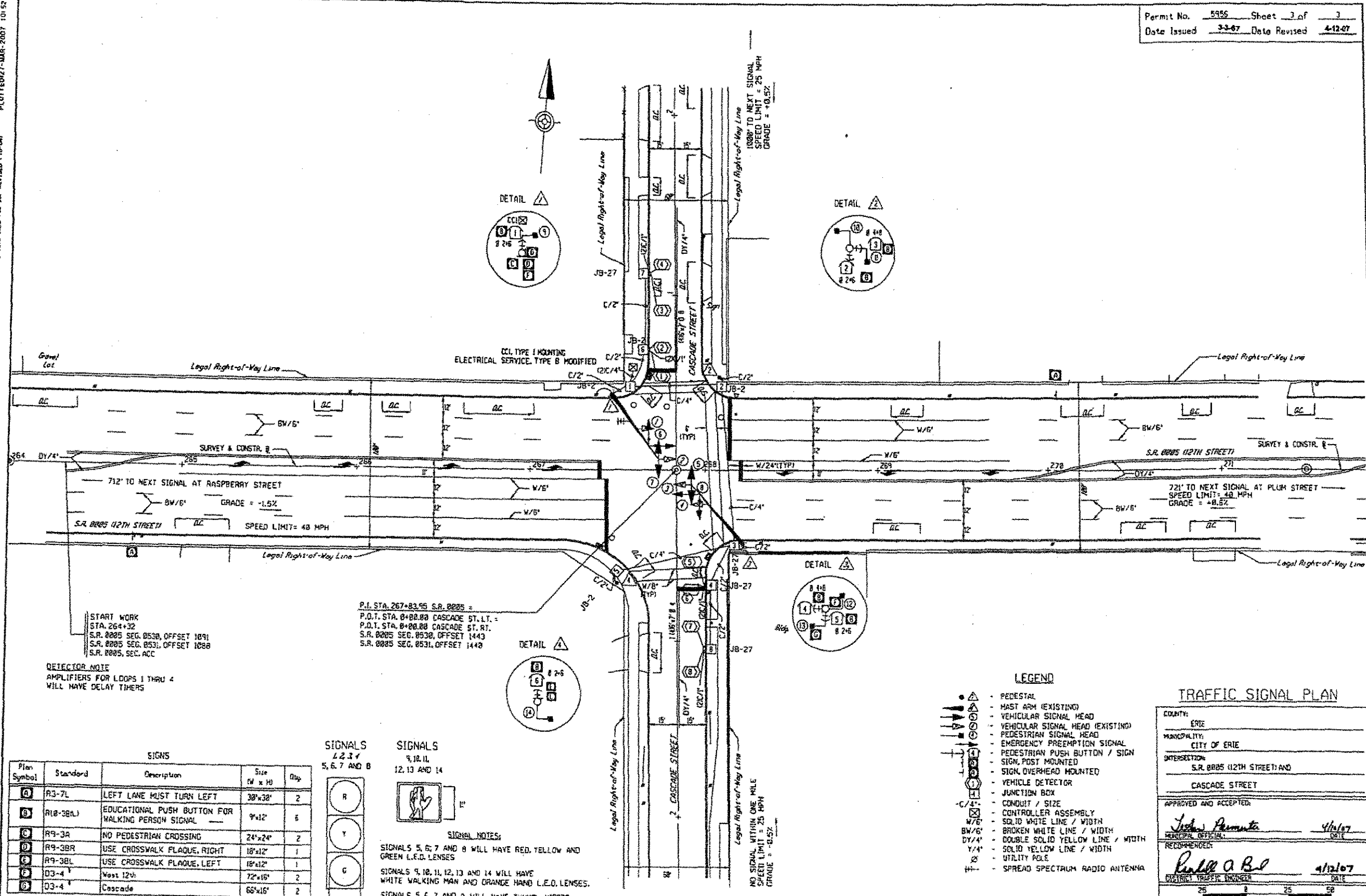
D-414

#4

Permit No. 5956 Sheet 3 of 3  
Date Issued 3-3-07 Date Revised 4-12-07

PLOTED 27-MAR-2007 10:52  
P-2012 CADS 102-300 REVISED 11-01-04

OPERATION: FILE NAME: L:\V2\Traffic\Signal\Permit Drawings\Traffic Signal\Permit\Traffic Signal\5th and Cascade 5L\_5eet 5 ACC.dgn

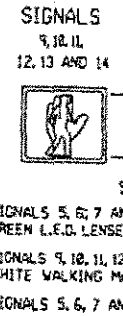
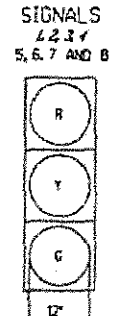


START WORK  
STA. 264+32  
S.R. 8005 SEC. 0530, OFFSET 1091  
S.R. 8005 SEC. 0531, OFFSET 1088  
S.R. 8005, SEC. ACC

DETECTOR NOTE  
AMPLIFIERS FOR LOOPS 1 THRU 4  
WILL HAVE DELAY TIMERS

P.I. STA. 267+83.95 S.R. 8005 =  
P.O.T. STA. 0+00.00 CASCADE ST. LT. =  
P.O.T. STA. 0+00.00 CASCADE ST. RT. =  
S.R. 8005 SEC. 0530, OFFSET 1443  
S.R. 8005 SEC. 0531, OFFSET 1448

Plan Symbol	Standard	Description	Size (W x H)	Qty
A	R3-7L	LEFT LANE MUST TURN LEFT	30"x30"	2
B	R18-38L	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	6
C	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
D	R9-3BR	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
E	R9-3BL	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
F	D3-4	West 12th	72"x18"	2
G	D3-4	Cascade	66"x18"	2



SIGNAL NOTES:  
SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.D. LENSES  
SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.  
SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.

- LEGEND
- ▲ PEDESTAL
  - ▲ HAST ARM (EXISTING)
  - VEHICULAR SIGNAL HEAD
  - VEHICULAR SIGNAL HEAD (EXISTING)
  - PEDESTRIAN SIGNAL HEAD
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTROLLER ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

TRAFFIC SIGNAL PLAN

COUNTY: ERIE

MUNICIPALITY: CITY OF ERIE

INTERSECTION: S.R. 8005 (12TH STREET) AND CASCADE STREET

APPROVED AND ACCEPTED:

*John Permut* 4/12/07  
MUNICIPAL OFFICIAL DATE

RECOMMENDED:

*Ronald A. Bell* 4/12/07  
DISTRICT TRAFFIC ENGINEER DATE

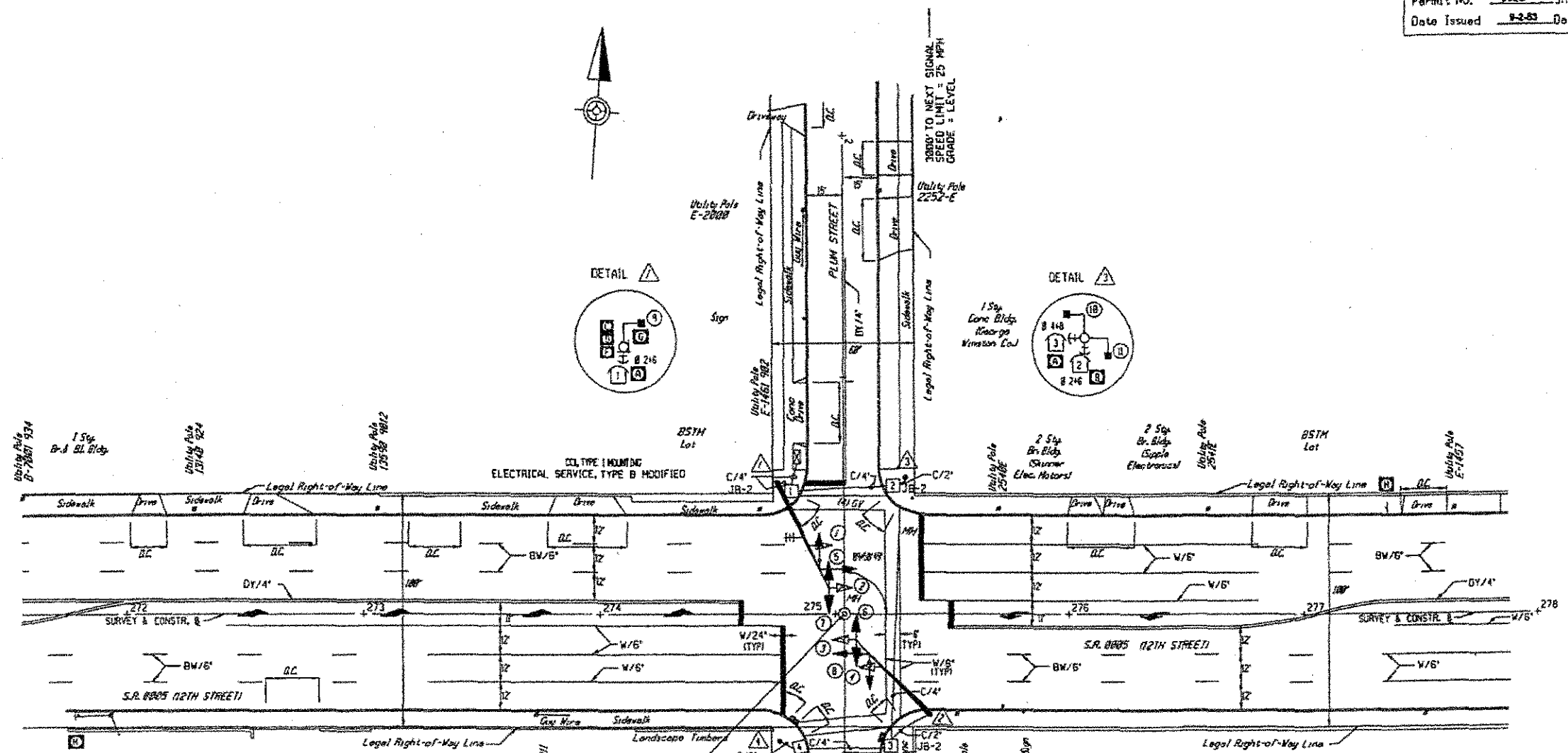
SCALE IN FEET

D-415

#5

Permit No. 3920 Sheet 1 of 3  
Date Issued 8-2-03 Date Revised 4-12-07

PLOTTED: MAR-2007 09:47  
 D-4012 CAD (02-04) REVISED 11-0-04  
 FILE NAME: L:\172\Traffic Signals\Permit Drawings\Traffic Signal\Traffic Signal\Plan St. 501 5 ACC.201.dwg



721' TO NEXT SIGNAL AT CASCADE STREET  
SPEED LIMIT = 48 MPH  
GRADE = -0.4%

P.I. STA. 275+23.90 S.R. 8805 =  
P.O.T. STA. 0+08.88 PLUM ST. LT. =  
P.O.T. STA. 0+08.88 PLUM ST. RT. =  
S.R. 8805 SEG. 0538 OFFSET 2165  
S.R. 8805 SEG. 0531 OFFSET 2162

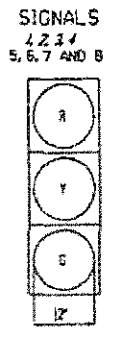
411' TO NEXT SIGNAL AT LIBERTY STREET  
SPEED LIMIT = 48 MPH  
GRADE = -0.5%

DETECTOR NOTE  
AMPLIFIERS FOR LOOPS 1 THRU 7  
WILL HAVE DELAY TIMERS

SIGNS

Plan Symbol	Standard	Description	Size (W x H)	Qty
(A)	R10-380A	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	2
(B)	R10-380B	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	4
(C)	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
(D)	R9-3B	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
(E)	R9-3B	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
(F)	D3-4	West 12th	72"x15"	2
(G)	D3-4	Plum	48"x15"	2
(H)	R3-7L	LEFT LANE MUST TURN LEFT	38"x38"	2

1 - WHITE REFLECTORIZED LEGEND ON GREEN REFLECTORIZED BACKGROUND



SIGNALS

1, 2, 3, 4  
5, 6, 7 AND 8



SIGNAL NOTES:

SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.  
SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.D. LENSES.  
SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.

LEGEND

- PEDESTAL
- MAST ARM (EXISTING)
- VEHICULAR SIGNAL HEAD
- VEHICULAR SIGNAL HEAD (EXISTING)
- PEDESTRIAN SIGNAL HEAD
- EMERGENCY PREEMPTION SIGNAL
- PEDESTRIAN PUSH BUTTON / SIGN
- SIGN, POST MOUNTED
- SIGN, OVERHEAD MOUNTED
- VEHICLE DETECTOR
- JUNCTION BOX
- CONDUIT / SIZE
- CONTROLLER ASSEMBLY
- SOLID WHITE LINE / WIDTH
- BROKEN WHITE LINE / WIDTH
- DOUBLE SOLID YELLOW LINE / WIDTH
- SOLID YELLOW LINE / WIDTH
- UTILITY POLE
- SPREAD SPECTRUM RADIO ANTENNA

TRAFFIC SIGNAL PLAN

COUNTY: ERIE  
 MUNICIPALITY: CITY OF ERIE  
 INTERSECTION: S.R. 8805 (12TH STREET) AND PLUM STREET

APPROVED AND ACCEPTED:  
 [Signature] 4/12/07  
 MUNICIPAL OFFICIALS

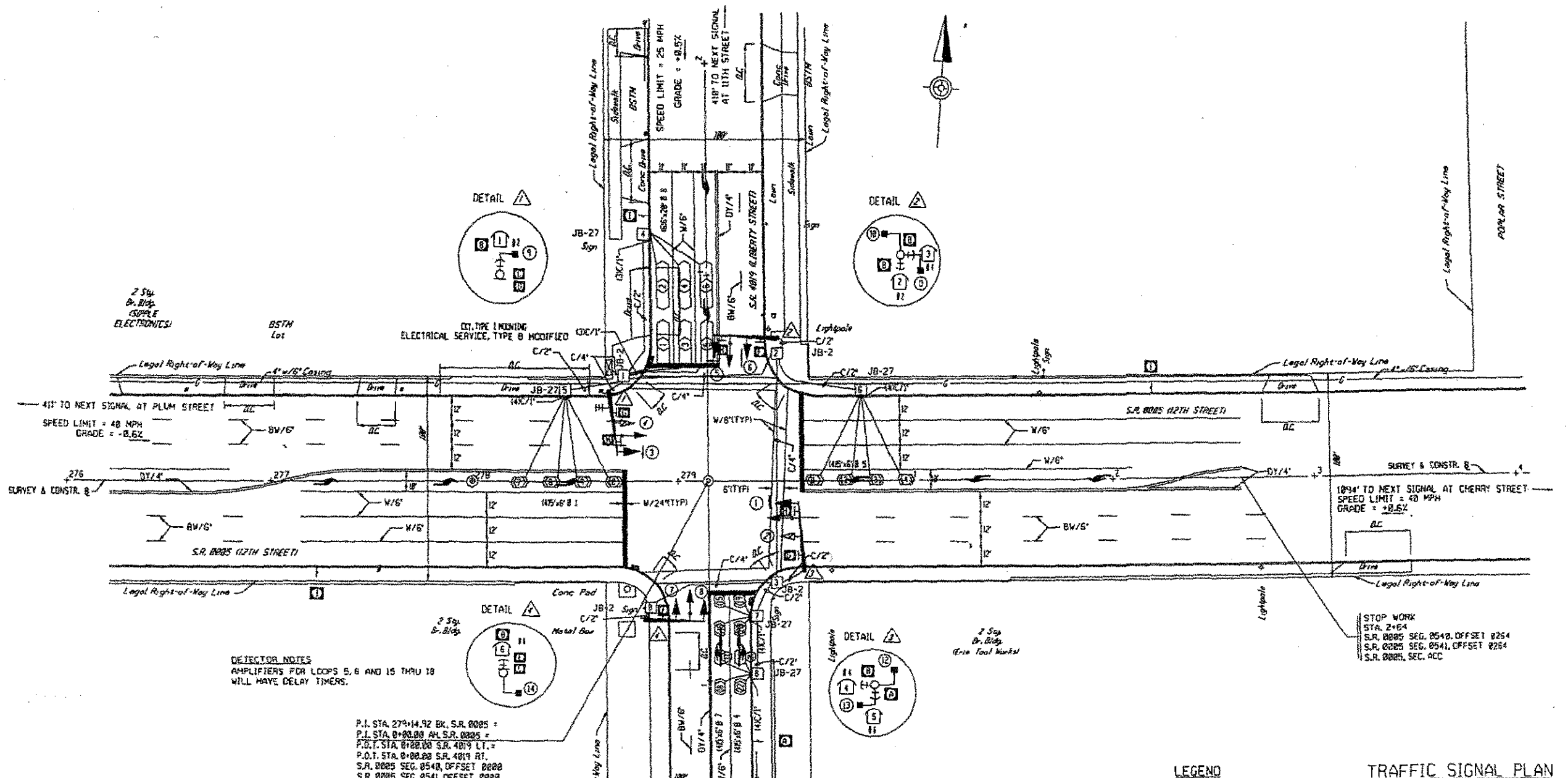
RECOMMENDED:  
 [Signature] 4/12/07  
 DISTRICT TRAFFIC ENGINEER DATE

SCALE IN FEET: 25 50

D-416

Permit No. 4488 Sheet 3 of 3  
 Date Issued 5-15-06 Date Revised 4-12-07

D:\1012 CAD\102-98 REVISED (10-04) PLOTTED 27-MAR-2007 14:51  
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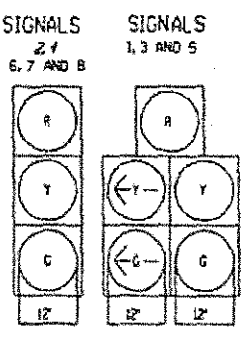


**DETECTOR NOTES**  
 AMPLIFIERS FOR LOOPS 5, 6 AND 15 THRU 18  
 WILL HAVE DELAY TIMERS.

P.I. STA. 279+14.92 BK. S.R. 0025 =  
 P.I. STA. 0+00.00 AL. S.R. 0025 =  
 P.O.T. STA. 0+00.00 S.R. 4819 LT. =  
 P.O.T. STA. 0+00.00 S.R. 4819 RT. =  
 S.R. 0025 SEC. 0540, OFFSET 0200  
 S.R. 0025 SEC. 0541, OFFSET 0200

STOP WORK  
 STA. 2+64  
 S.R. 0025 SEC. 0540, OFFSET 0254  
 S.R. 0025 SEC. 0541, OFFSET 0254  
 S.R. 0025, SEC. ACC

SIGNS				
Plan Symbol	Standard	Description	Size (W x H)	Qty
A	R3-8A SPECIAL (LS-SR)	LANE USE CONTROL (TWO LANES)	42"x36"	1
B	R10-38(L)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	6
C	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
D	R9-3BR	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
E	R9-3BL	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
F	D3-4 #	West 12th	72"x16"	2
G	D3-4 #	Liberty	68"x16"	2
H	R10-12	LEFT TURN YIELD ON GREEN	30"x36"	3
I	R3-7L	LEFT LANE MUST TURN LEFT	30"x30"	3



**SIGNALS**  
 2 / 6, 7 AND 8

**SIGNALS**  
 1, 3 AND 5

**SIGNALS**  
 9, 11, 12, 13 AND 14

**SIGNAL NOTES:**  
 SIGNALS 1, 3, 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.D. LENSES (INCLUDING ARROWS)  
 SIGNALS 1, 3, 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS  
 SIGNALS 9, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.

- LEGEND**
- MAST ARM (EXISTING)
  - VEHICULAR SIGNAL HEAD (EXISTING)
  - VEHICULAR SIGNAL HEAD
  - PEDESTRIAN SIGNAL HEAD
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTROLLER ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

**TRAFFIC SIGNAL PLAN**

COUNTY: ERIE

MUNICIPALITY: CITY OF ERIE

INTERSECTION: S.R. 0025 (12TH STREET) AND S.R. 4819 (LIBERTY STREET)

APPROVED AND ACCEPTED:  
*Richard A. B. P.* 4/10/07  
 DISTRICT TRAFFIC ENGINEER DATE

RECOMMENDED:  
*Richard A. B. P.* 4/12/07  
 DATE

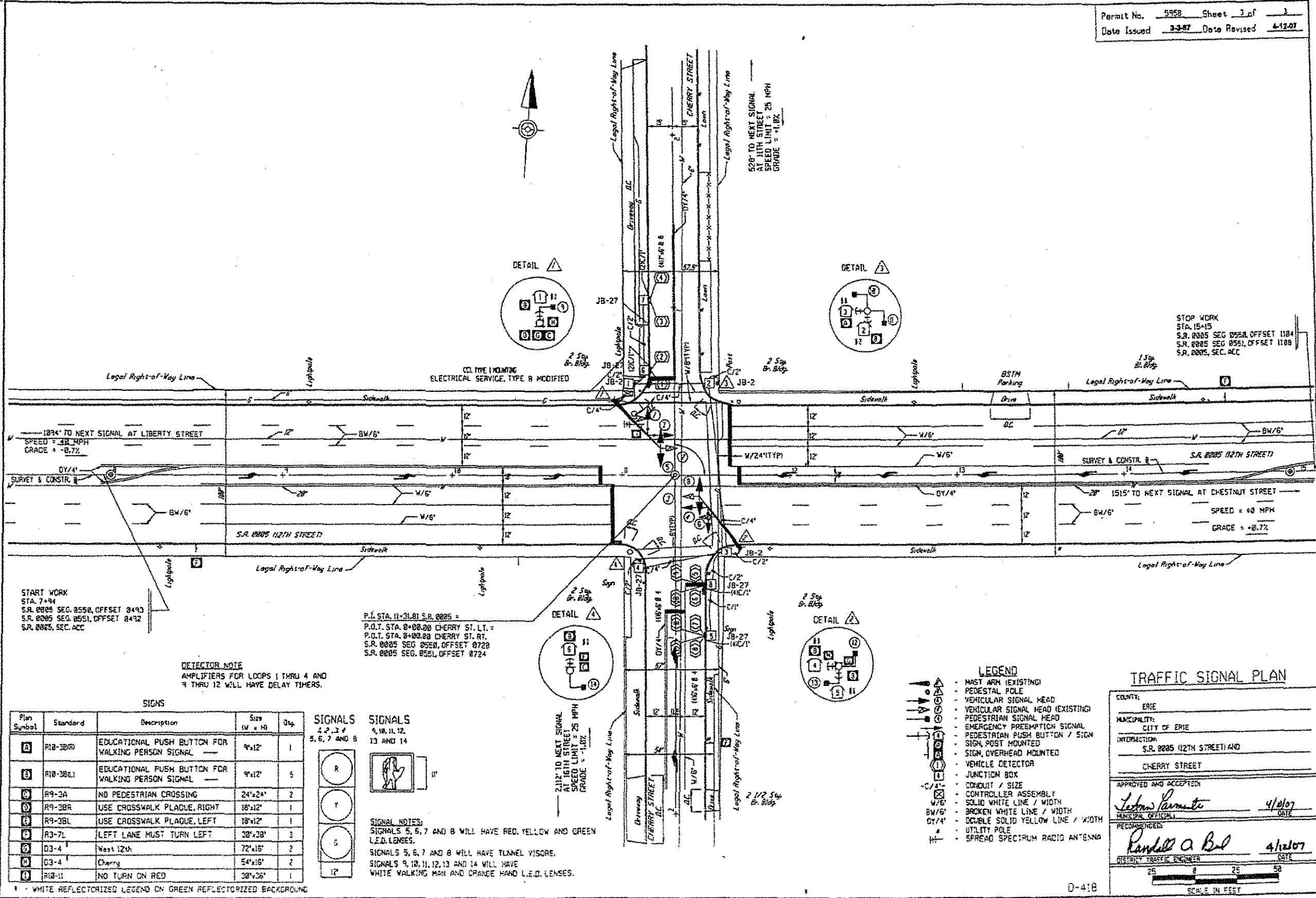
SCALE IN FEET  
 0 25 50

D-417

#8

Permit No. 5958 Sheet 3 of 3  
 Date Issued 3-3-07 Date Revised 4-12-07

OPERATOR: FILE NAME: L:\Traffic\Permit Drawings\Traffic Signal\Eriette\Traffic Signal 5 ACC.2D.dgn  
 PLOT: 02/27/07 14:52  
 D:\022 608 10-00 REVISED 10-04  
 PLOT: 02/27/07 14:52



**SIGNS**

Plan Symbol	Standard	Description	Size (W x H)	Qty
ⓐ	R10-3BR	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	1
ⓑ	R10-3BL	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	5
ⓒ	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
ⓓ	R9-3BR	USE CROSSWALK PLACQUE, RIGHT	18"x12"	1
ⓔ	R9-3BL	USE CROSSWALK PLACQUE, LEFT	18"x12"	1
ⓕ	R3-7L	LEFT LANE MUST TURN LEFT	30"x30"	3
ⓖ	O3-4	West 12th	72"x15"	2
ⓗ	O3-4	Cherry	54"x15"	2
ⓓ	R10-11	NO TURN ON RED	30"x36"	1

1 - WHITE REFLECTORIZED LEGEND ON GREEN REFLECTORIZED BACKGROUND

**SIGNALS**

Signal	Location
1, 2, 3, 4	S.R. 0005 12th Street
5, 6, 7 AND 8	Cherry Street

**SIGNAL NOTES:**  
 SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.D. LENSES.  
 SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.  
 SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.

- LEGEND**
- MAST ARM (EXISTING)
  - PEDESTAL POLE
  - VEHICULAR SIGNAL HEAD
  - VEHICULAR SIGNAL HEAD (EXISTING)
  - PEDESTRIAN SIGNAL HEAD
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTROLLER ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

**TRAFFIC SIGNAL PLAN**

COUNTY: ERIE  
 MUNICIPALITY: CITY OF ERIE  
 INTERSECTION: S.R. 0005 12TH STREET AND CHERRY STREET

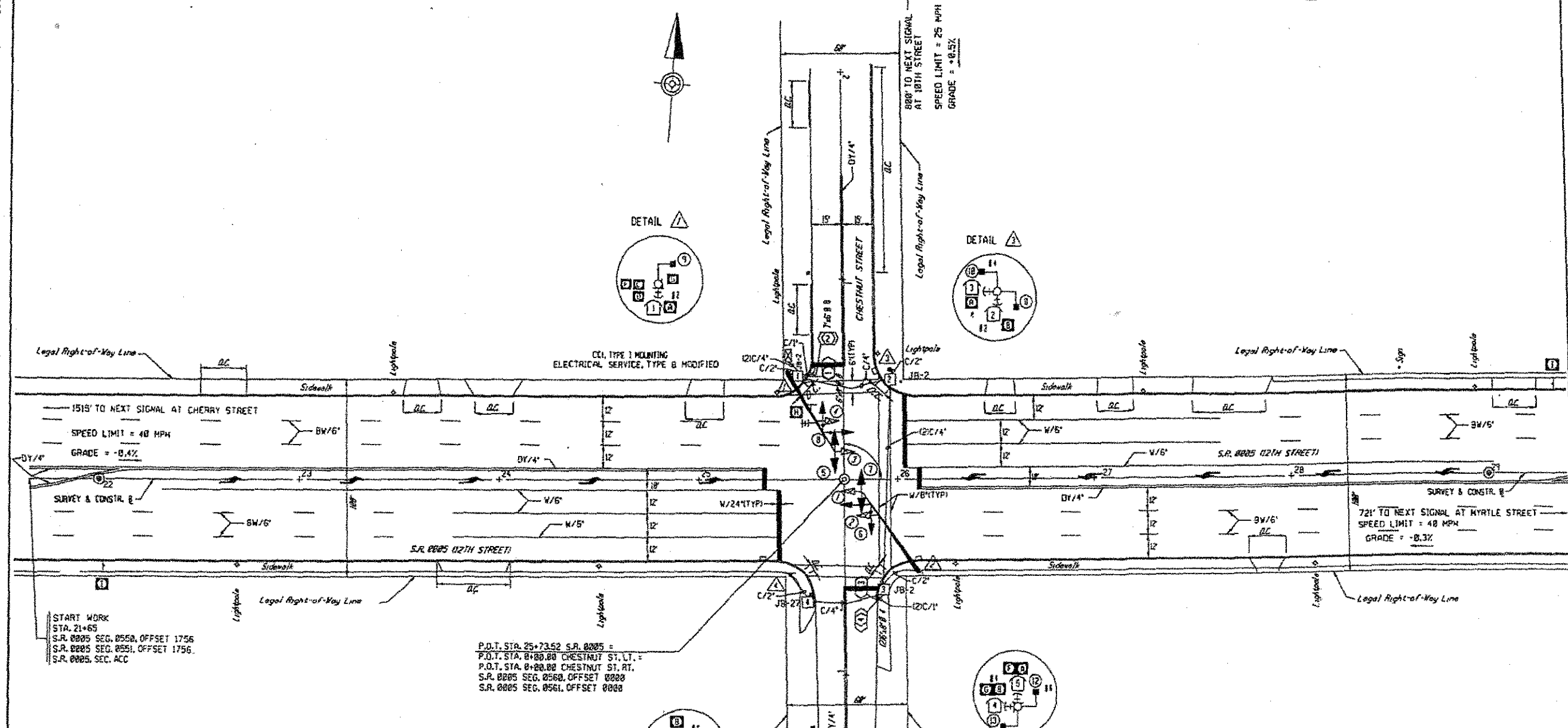
APPROVED AND ACCEPTED:  
*John Parmito* 4/10/07  
 MUNICIPAL OFFICIAL DATE

RECOMMENDED:  
*Randall A. Bel* 4/12/07  
 DISTRICT TRAFFIC ENGINEER DATE

SCALE IN FEET  
 0 25 50

Permit No. 5959 Sheet 3 of 3  
Date Issued 3-3-07 Date Revised 4-12-07

OPERATOR: FILE NAME: \\L:\V\Traffic\Signs\Traffic\_Signals\Traffic\_Signals\Traffic\_Signals\_1812th and Chestnut St. Seat 5 Acc.7D.dgn  
 PLOTTED: 27-MAR-2007 10:47  
 0:0012 CADW 102-306 REVISED 10-04



SIGNS

Plan Symbol	Standard	Description	Size (W x H)	Qty.
A	R18-38(R)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	3
B	R18-38(L)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	3
C	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
D	R9-38R	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
E	R9-38L	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
F	D3-4	West 12th	72"x18"	2
G	D3-4	Chestnut	66"x18"	2
H	R18-11	NO TURN ON RED	30"x36"	1
I	R3-7L	LEFT LANE MUST TURN LEFT	30"x36"	2

1 - WHITE REFLECTORIZED LEGEND ON GREEN REFLECTORIZED BACKGROUND

SIGNALS 5, 6, 7 AND 8

Signal No.	Location	Notes
5, 6, 7 AND 8	12TH STREET	SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.O. LENSES.
5, 6, 7 AND 8	CHESTNUT STREET	SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.
9, 10, 11, 12, 13 AND 14	CHESTNUT STREET	SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.O. LENSES.

SIGNAL NOTES:  
SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN L.E.O. LENSES.  
SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.  
SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.O. LENSES.

- LEGEND
- MAST ARM (EXISTING)
  - PEDESTAL POLE
  - VEHICULAR SIGNAL HEAD
  - PEDESTRIAN SIGNAL HEAD (EXISTING)
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTROLLER ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

TRAFFIC SIGNAL PLAN

COUNTY: ERIE

MUNICIPALITY: CITY OF ERIE

INTERSECTION: S.R. 8005 12TH STREET AND CHESTNUT STREET

APPROVED AND ACCEPTED:  
  
 DISTRICT TRAFFIC ENGINEER 4/10/07 DATE

RECOMMENDED:  
  
 DISTRICT TRAFFIC ENGINEER 4/12/07 DATE

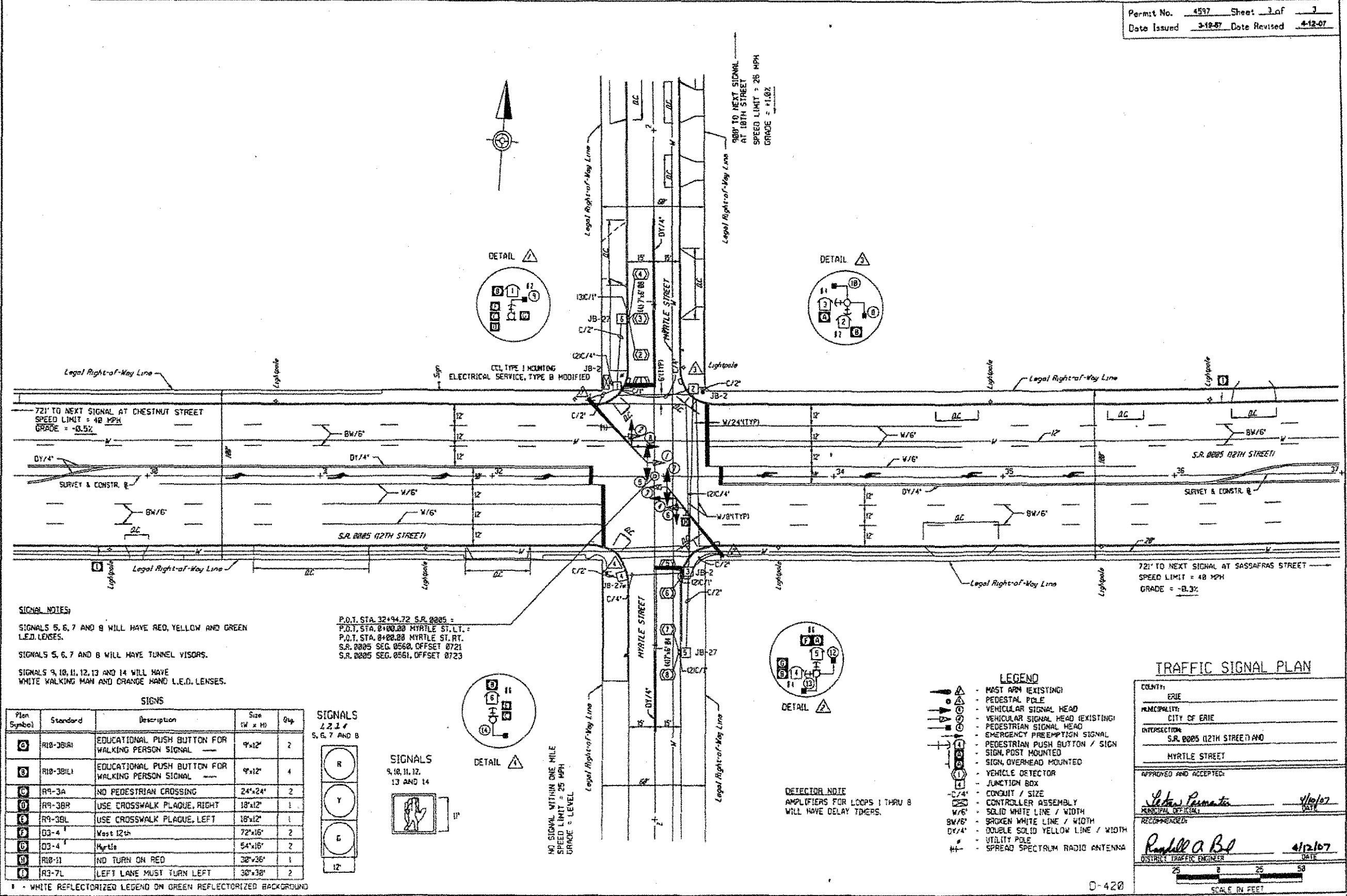
SCALE IN FEET

D-419

#11

Permit No. 4597 Sheet 3 of 3  
 Date Issued 3-12-07 Date Revised 4-12-07

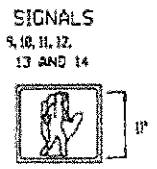
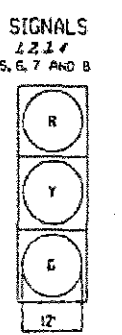
OPERATOR: FILE NAME: L:\Traffic\Signal\Permit Drawings\Traffic Signal\Erie\City of Erie\12th and Myrtle St. Sheet 5 ACC.DWG  
 PLOTTED: 27-MAR-2007 14:37  
 D:\072 C00 (07-03) REVISED (10-04)



**SIGNAL NOTES:**  
 SIGNALS 5, 6, 7 AND 8 WILL HAVE RED, YELLOW AND GREEN LED LENSES.  
 SIGNALS 5, 6, 7 AND 8 WILL HAVE TUNNEL VISORS.  
 SIGNALS 9, 10, 11, 12, 13 AND 14 WILL HAVE WHITE WALKING MAN AND ORANGE HAND L.E.D. LENSES.

P.O.T. STA. 32+94.72 S.R. 8805 =  
 P.O.T. STA. 0+00.00 MYRTLE ST. L.T. =  
 P.O.T. STA. 0+00.00 MYRTLE ST. RT. =  
 S.R. 8805 SEC. 0560, OFFSET 0721  
 S.R. 8805 SEC. 0561, OFFSET 0723

Plan Symbol	Standard	Description	Size (W x H)	Qty
ⓐ	R18-38(A)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	2
ⓑ	R18-38(B)	EDUCATIONAL PUSH BUTTON FOR WALKING PERSON SIGNAL	9"x12"	4
ⓒ	R9-3A	NO PEDESTRIAN CROSSING	24"x24"	2
ⓓ	R9-3B	USE CROSSWALK PLAQUE, RIGHT	18"x12"	1
ⓔ	R9-3B	USE CROSSWALK PLAQUE, LEFT	18"x12"	1
ⓕ	D3-4	West 12th	72"x16"	2
ⓖ	D3-4	Myrtle	54"x18"	2
ⓗ	R10-11	NO TURN ON RED	32"x36"	1
ⓘ	R3-7L	LEFT LANE MUST TURN LEFT	30"x30"	2



NO SIGNAL WITHIN ONE MILE  
 SPEED LIMIT = 25 MPH  
 GRADE = LEVEL

- LEGEND**
- MAST ARM (EXISTING)
  - PEDESTAL POLE
  - VEHICULAR SIGNAL HEAD
  - VEHICULAR SIGNAL HEAD (EXISTING)
  - PEDESTRIAN SIGNAL HEAD
  - EMERGENCY PREEMPTION SIGNAL
  - PEDESTRIAN PUSH BUTTON / SIGN
  - SIGN, POST MOUNTED
  - SIGN, OVERHEAD MOUNTED
  - VEHICLE DETECTOR
  - JUNCTION BOX
  - CONDUIT / SIZE
  - CONTROLLER ASSEMBLY
  - SOLID WHITE LINE / WIDTH
  - BROKEN WHITE LINE / WIDTH
  - DOUBLE SOLID YELLOW LINE / WIDTH
  - UTILITY POLE
  - SPREAD SPECTRUM RADIO ANTENNA

**TRAFFIC SIGNAL PLAN**

COUNTY: ERIE

MUNICIPALITY: CITY OF ERIE

INTERSECTION: S.R. 8805 (12TH STREET) AND MYRTLE STREET

APPROVED AND ACCEPTED:

*John P. ...* 4/10/07  
 MUNICIPAL OFFICIAL DATE

RECOMMENDED:

*Randall A. ...* 4/12/07  
 DISTRICT TRAFFIC ENGINEER DATE

SCALE IN FEET: 0 25 50

D-420

PERMIT NO. TEMPORARY SHEET 2 OF 2  
 DATE ISSUED 10-4-89 DATE REVISED

# TEMPORARY TRAFFIC SIGNAL

PHASE	INTERVALS			INTERVALS			EMERGENCY FLASHING ONLY
	1	2	3	1	2	3	
SIGNALS	1	2	3	1	2	3	
1,2,5,6	G	Y	R	R	R	R	Y
3,4,7,8	R	R	R	G	Y	R	R
FIXED	40	4	2	28	4	2	
MINIMUM PASSAGE							
MAXIMUM							
MEMORY	MX			MX			

MX - MAXIMUM RECALL

**GENERAL NOTES**

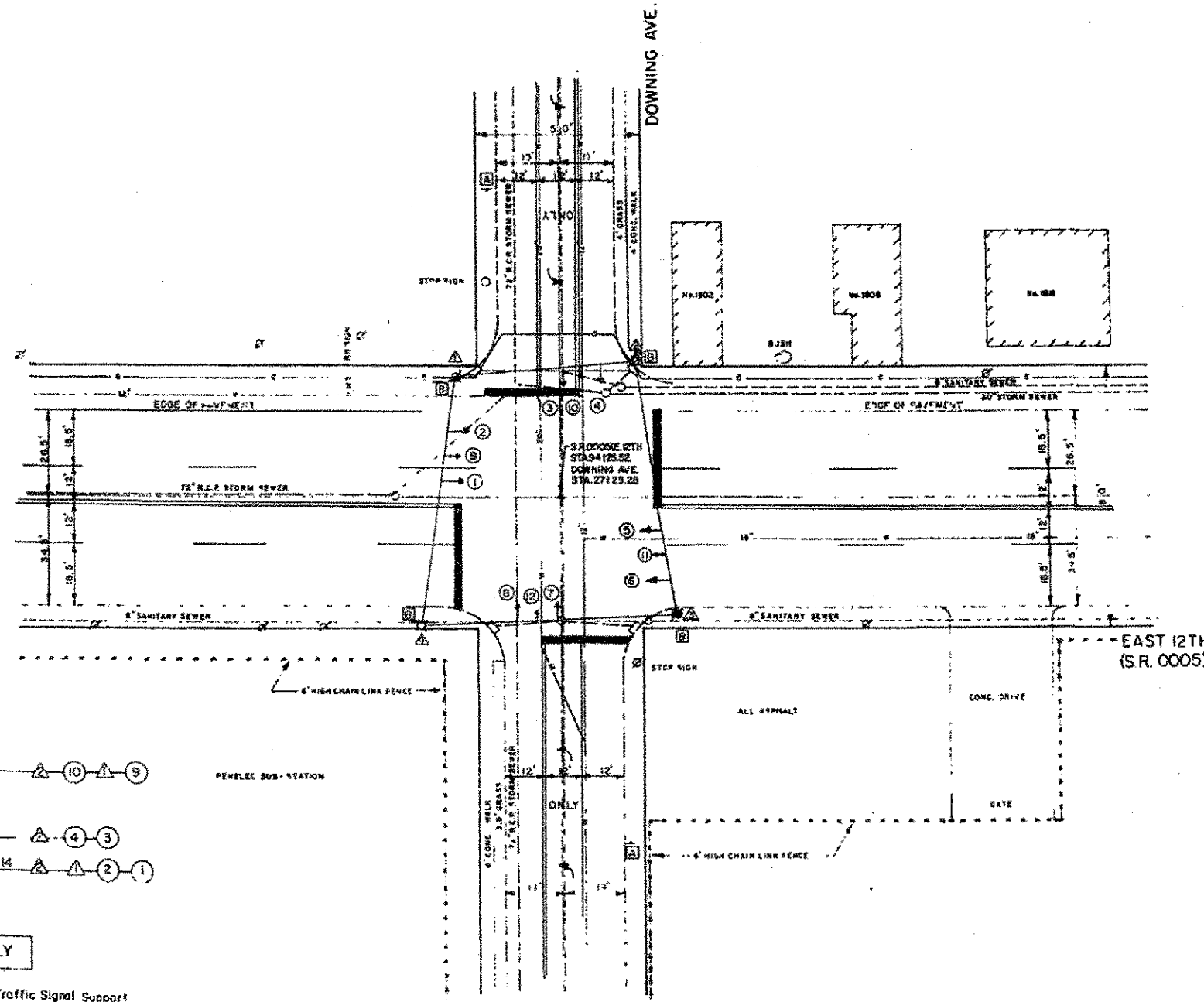
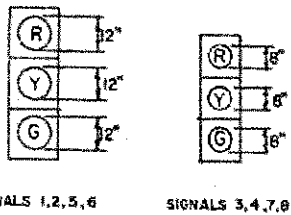
Install, operate and maintain these signals in accordance with the Pennsylvania Department of Transportation Regulations that govern the design, location and operation of signs, signals and markings.

Mount signal heads a minimum of 17 ft. above the roadway.

Install Type 'B' electrical service on pole no. 2.



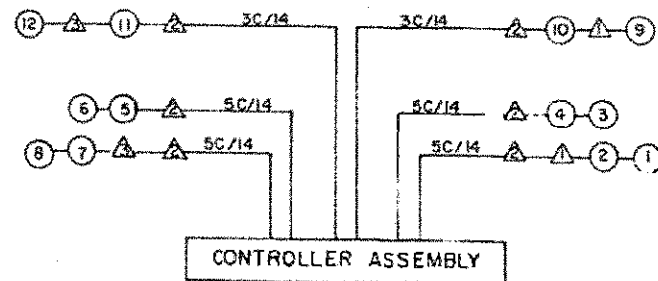
**EMERGENCY VEHICLE PREEMPTION NOTE**  
 SUPPLY AN EMERGENCY VEHICLE PREEMPTION UNIT WHICH ACTIVATES THE INTERNAL CONTROLLER PREEMPTION SEQUENCES. UPON RECEIVING A SIGNAL FROM AN EMERGENCY VEHICLE, CAUSE THE CONTROLLER, IF IN THE "GREEN" INTERVAL OF A CONFLICTING PHASE AND UTILIZING CONTROLLER INTERVAL TIMINGS, TO PROVIDE A "YELLOW" CHANGE INTERVAL AND AN "ALL RED" INTERVAL, RESULTING IN THE SELECTED STREET BECOMING "GREEN" WITH ALL OTHERS REMAINING "RED". IF PREEMPTED DURING ANY OTHER INTERVAL TIME OUT THE INTERVAL AND PROCEED AS DESCRIBED ABOVE. PROVIDE A PREEMPTION INDICATION TO THE SELECTED STREET WITH THE GREEN. UPON TERMINATION OF PREEMPTION SIGNAL RETURN TO NORMAL OPERATION.



**SIGN TABULATION**

PLAN SYMBOL	DESCRIPTION	SIZE W x H
A	R3-7L LEFT LANE MUST TURN LEFT	30" x 30"
B	R9-3 NO PEDESTRIAN CROSSING	18" x 18"

**WIRING DIAGRAM**



- - Signal Head
  - △ - Traffic Signal Support
  - ⊖ - Detector
  - - Junction Box
- 5C/14 - Cable (No of Conductors/Size AWG)

**LEGEND**

- △ - Mast Arm
- △ - Signal Pole
- - Poletop
- - Vehicular Signal Head
- - Pedestrian Signal Head
- - Stop
- - Vehicle Detector
- - Junction Box
- - Pedestrian Post Support/Stop
- CC - Controller Assembly
- 4A - Solid White Line/Width
- 4WA - Broken White Line/Width
- 4Y - Solid Yellow Line/Width
- 4YA - Broken Yellow Line/Width
- 4DA - Double Solid White Line/Width
- 4DU - Centering Utility Pole
- - Emergency Vehicle Preempt Confirmation Light
- - Control/Size

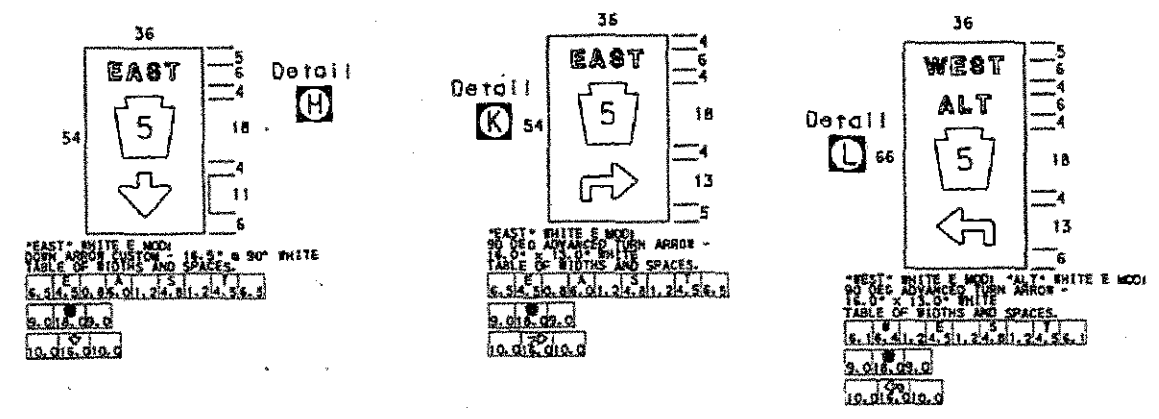
County  
**ERIE**  
 Municipality  
**CITY OF ERIE**  
 Intersection of  
**S.R.0005(E 12TH ST) &**  
**DOWNING AVENUE**  
 Reviewed:  
*Richard J. Morris*  
 Municipal Official  
 Recommended:  
*[Signature]*  
 District Traffic Engineer  
 Date  
 10/4/89  
 Scale

D712



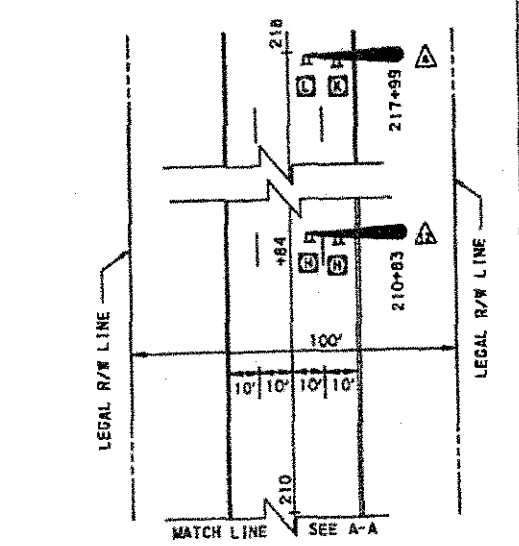
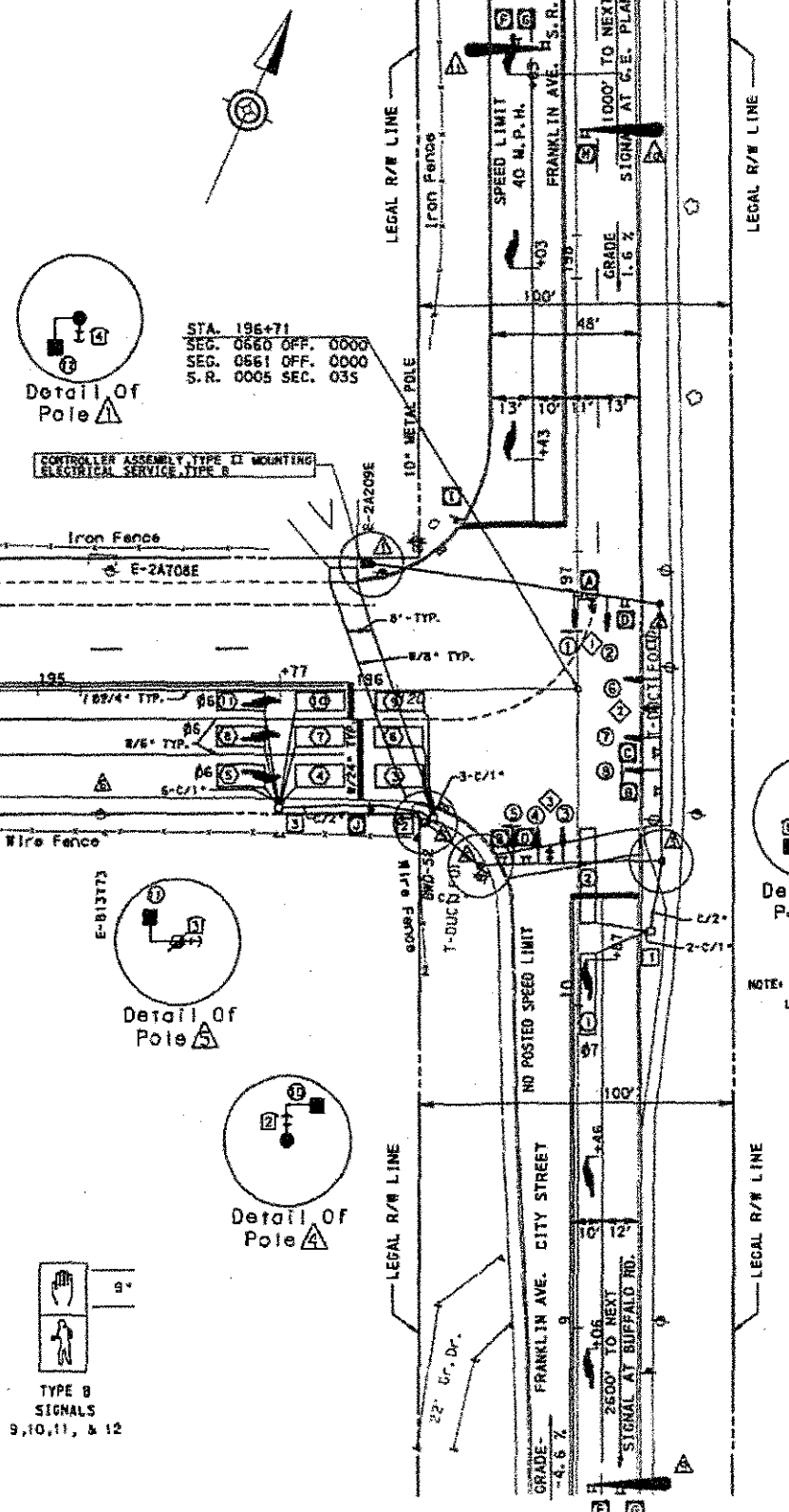
PLOTTED: 19-JUL-2005 09:12

PERMIT NO. : 1401 SHEET : 2 OF 3  
 DATE ISSUED : 9-12-47 DATE REVISED : 1-23-06



NOTE: ALL SPECIAL SIGNS HAVE 2.0" RADIUS, 0.8" BORDER, BLACK ON WHITE

NOTE: LOOPS ①, ②, ③ - 7' x 15' AND  
 ④, ⑤, ⑥, ⑦, ⑧, ⑨, ⑩, ⑪ - 6' x 15'



**LEGEND**

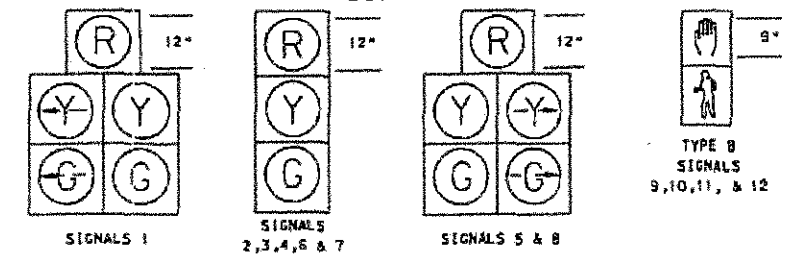
- ▲ - MASTARM SIGNAL & SIGN SUPPORT
- - STRAIN POLE/MESSENGER CABLE
- - VEHICULAR SIGNAL HEAD
- - PEDESTRIAN SIGNAL HEAD
- - OVERHEAD MOUNTED SIGN
- - POST MOUNTED SIGN
- - VEHICLE DETECTOR
- - PEDESTRIAN PUSH BUTTON/SIGN
- - CONTROLLER ASSEMBLY
- - JUNCTION BOX
- - CONDUIT/SIZE
- - SOLID WHITE LINE/WIDTH
- - BROKEN WHITE LINE/WIDTH
- - SOLID YELLOW LINE/WIDTH
- - BROKEN YELLOW LINE/WIDTH
- - DOUBLE SOLID YELLOW LINE/WIDTH
- - UTILITY POLE
- - EMERGENCY VEHICLE PREEMPTION
- - DEPRESSED CURB

NOTE: LOOPS ① - 5' x 25'  
 ② - 5' x 30'

**REQUIRED SIGNS**

PLAN SYMBOL	DESCRIPTION & STANDARD	SIZE (WIDTH X HEIGHT)
A	R10-12 LEFT TURN YIELD ON GREEN	30" x 36"
B	R10-10R RIGHT TURN SIGNAL	30" x 36"
C	D3-4 FRANKLIN	66" x 16"
D	D3-4 EAST 12th	66" x 16"
E	R3-SL LEFT TURN	30" x 36"
F	R3-SR RIGHT TURN	30" x 36"
G	R3-S5 STRAIGHT-THROUGH ONLY	30" x 36"
H	SPECIAL SIGN, SEE DETAIL H	34" x 54"
I	R3-7R RIGHT LANE MUST TURN RIGHT	30" x 30"
J	R3-8LLR LANE USE CONTROL	48" x 40"
K	SPECIAL SIGN, SEE DETAIL K	36" x 54"
L	SPECIAL SIGN, SEE DETAIL L	36" x 66"
M	R10-4B PUSH BUTTON FOR WALKING PERSON	9" x 12"

**SIGNALS**



SIGNALS 1 THRU 8 EQUIPPED WITH TUNNEL VISORS

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION  
 DISTRICT I-0 TRAFFIC UNIT

COUNTY : ERIE  
 MUNICIPALITY : CITY OF ERIE  
 EAST 12TH STREET (S.R. 0005)  
 AND FRANKLIN AVENUE

APPROVED AND ACCEPTED:  
*[Signature]*  
 MUNICIPAL OFFICIAL DATE 1-23-06

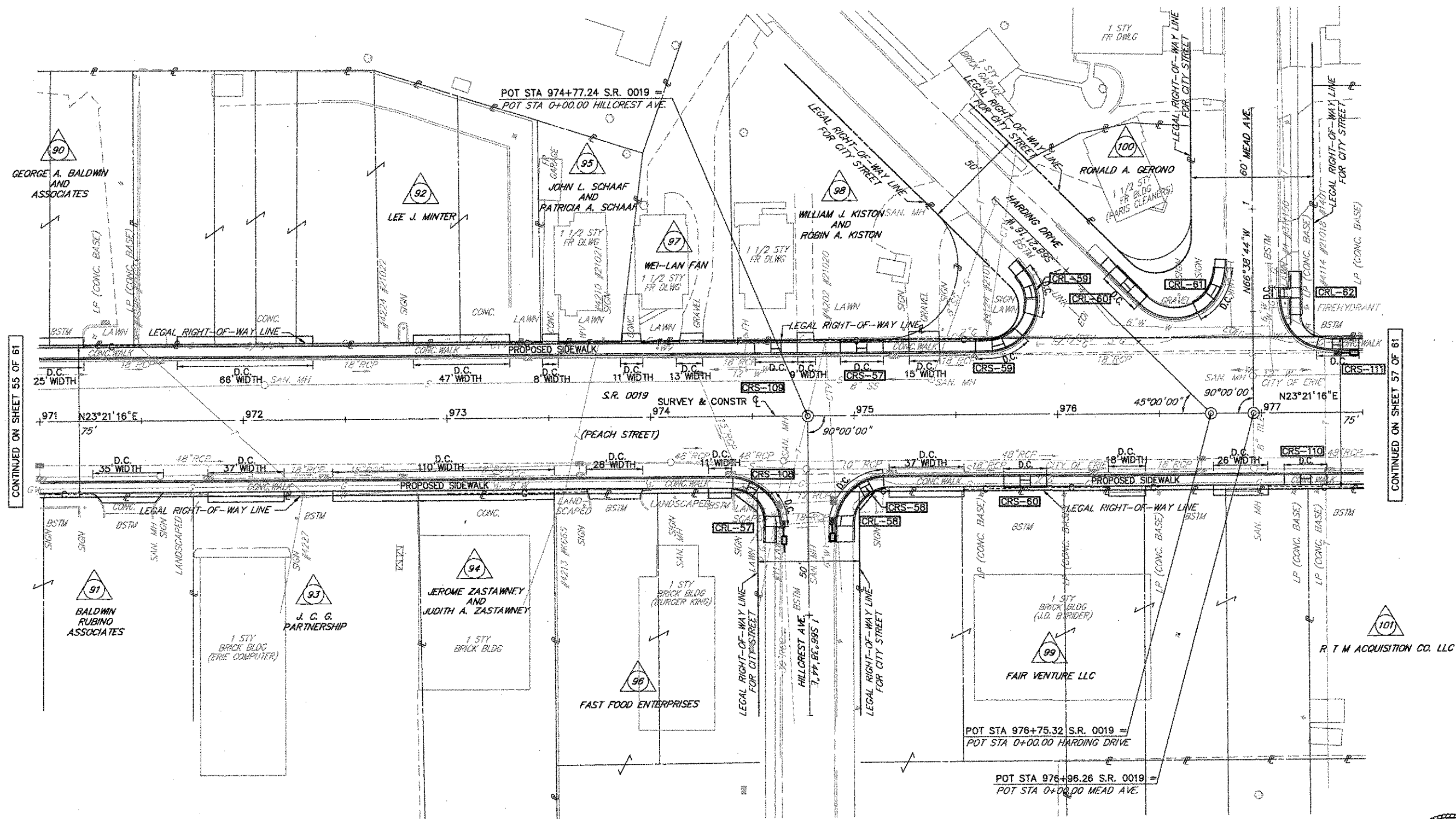
RECOMMENDED BY:  
*[Signature]*  
 DISTRICT TRAFFIC ENGINEER DATE 1-23-06

25 0 25 DRAWING NO. D-254

OPERATOR: FILE NAME: L:\132\Permit Drawings\Traffic Signal\er12mfrkfrk.dwg

BM ELEV B25.10  
60.0 RT STA 974+45.0 SURVEY & CONSTR  $\nabla$   
SHOT #41  $\square$  CUT IN CONC. BASE

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
1-0	ERIE	0019	A08	56 OF 61
CITY OF ERIE				
REVISION NUMBER	REVISIONS	DATE	BY	



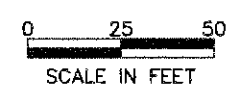
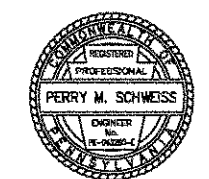
CONTINUED ON SHEET 55 OF 61

CONTINUED ON SHEET 57 OF 61

**LEGEND**

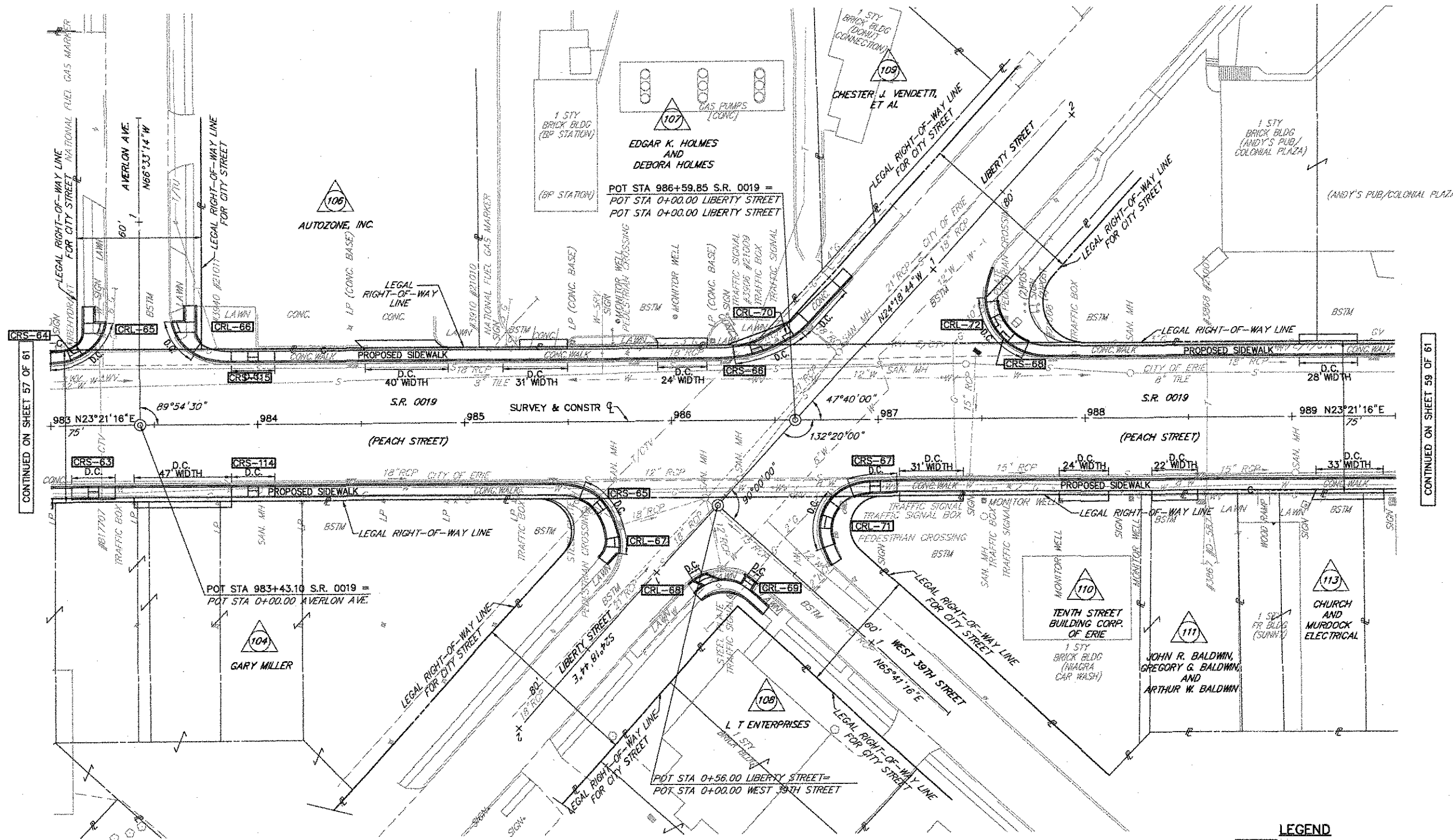
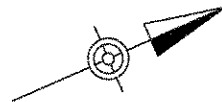
CRS-# CURB RAMP STATE

CRL-# CURB RAMP LOCAL



DISTRICT	COUNTY	ROUTE	SECTION	SHEET
1-0	ERIE	0019	A08	58 OF 61
CITY OF ERIE				
REVISION NUMBER	REVISIONS	DATE	BY	

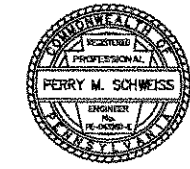
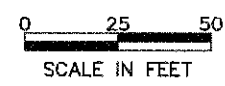
BM ELEV 815.81  
 41.0' LT STA 986+30.5 SURVEY & CONSTR  $\phi$   
 SHOT #45 "C" CUT IN CONC BASE



CONTINUED ON SHEET 57 OF 61

CONTINUED ON SHEET 59 OF 61

**LEGEND**  
 CRS-# CURB RAMP STATE  
 CRL-# CURB RAMP LOCAL



TE-672 (9-10)

**PEDESTRIAN ACCOMMODATION  
AT INTERSECTIONS CHECKLIST**



PLEASE TYPE OR PRINT ALL INFORMATION IN BLUE OR BLACK INK

As specified in the MUTCD, Section 4E, an engineering study shall be conducted to determine the need for pedestrian accommodation at signalized intersections and the related design and operational features. Based on the engineering study and engineering judgment, proper documentation shall be made at all new signalized intersections and modifications to existing signalized intersections. This documentation shall be provided with guidance from this checklist.

When pedestrian accommodations will not be provided at an intersection, signalized or not, proper justification must be documented. Below is a checklist of information that may be relevant in the determination of pedestrian needs and warranted accommodations at an intersection. Not all of the information below is required to make a determination. This form can be used to summarize the needs and accommodations of a corridor or a single intersection.

Intersection Background Information				
DATE February 1, 2013	DISTRICT 1-0	COUNTY Erie		
MUNICIPALITY City of Erie		INTERSECTION SR 0019 & Walker		
SUBMITTED BY Paul A. Miller, P.E.		STREET ADDRESS 255 Elm Street		
CITY Oil City	STATE PA	ZIP 16301	TELEPHONE NUMBER 814-678-7107	
Project Overview				
Provide a Project Description and Scope of the Project.		Curb ramp and sidewalk project to update missing and non-compliant ramps and sidewalk		
Bike/Ped Checklist Completed. <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No				
Existing Facility Description				
Yes	No			
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pedestrian facilities present (i.e., sidewalks, curb ramps, crosswalks, pedestrian signals, etc.)?		
<input type="checkbox"/>	<input type="checkbox"/>	Are there descriptions of each quadrant of the existing intersection (photos are strongly recommended)?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are near-by land uses documented?		
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pedestrian facilities near the intersection (i.e., sidewalks, bus stops, trails, etc.)?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is there current evidence of pedestrians using the intersection (worn paths, observed activity)?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Were special accommodations made in the past for pedestrians at the intersection?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are restrictions for pedestrians present? (No Ped signs, limited crossings)		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is this a defined walking route or safe route to school route?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are physical restrictions or right-of-way restrictions present?		
<input checked="" type="checkbox"/>	<input type="checkbox"/>	If pedestrian signals are present, is proper traffic signal timing designated for pedestrians at the intersection? PEACH / 38TH		
Proposed Facility Description				
Yes	No			
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Will the proposed improvements generate new or additional pedestrian traffic?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Does the proposed facility introduce possible additional restrictions for pedestrians?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are new or existing pedestrian signals proposed at the intersection?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are sidewalks proposed as part of the project?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are detailed descriptions of changes to each quadrant documented?		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Do near-by land uses change as part of the project?		
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are additional pedestrian facilities proposed for the intersection?		
Outreach Efforts				
Has contact and discussion concerning pedestrian accommodations at the intersection been made with the following?				
Yes	No			
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Municipality (s)		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Transit Organization (s)		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	School District (s)		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Meeting (s)		
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Emergency Services		
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Advocacy Groups		
<input type="checkbox"/>	<input type="checkbox"/>	Other (s) Curb ramp and sidewalk project to update missing and non-compliant ramps		

TE-672 (9-10)

**Intersection Details**

(The information below may be useful in the determination of pedestrian needs and warranted accommodations at uncontrolled intersections. Not all of the information is required to make a determination. The minor roadway information may also be needed in some situations.)

Roadway classification: Urban Regional Arterial  
 Roadway Typology: Suburban Center  
 Speed Limit (MPH): 40  
 Design Speed: 40  
 ADT: 21,721  
 Percentage of Trucks: 8  
 Travel lanes: 4 with left turn lane

Is there a sight distance deficiency?  Yes  No

If yes, explain. \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

Sidewalk:  Yes  No      Shoulders:  Yes  No      Curb:  Yes  No

Is parking permitted on the roadway?  Yes  No

Municipal recommendation: \_\_\_\_\_  
 \_\_\_\_\_

Pedestrian generators? Country Fair / Citizen's Bank

Distance to next available crossing: 150'

Was a crash analysis completed?  Yes  No

Are there pedestrian crashes?  Yes  No 2, NEITHER CRASH RELATED TO CROSSING AT WALKER.

Has a pedestrian gap analysis been performed at the location?  Yes  No

[ftp://ftp.dot.state.pa.us/transfer/Traffic Signals/Unsignalized Intersection Ped Calcs from HCS.xlsx](ftp://ftp.dot.state.pa.us/transfer/Traffic%20Signals/Unsignalized%20Intersection%20Ped%20Calcs%20from%20HCS.xlsx)

Are there other geometric concerns? If yes, explain: The intersection of Walker Blvd. is approximately 150' south of the 38th Street intersection which is currently being upgraded with new ADA compliant pedestrian facilities. Attached is a copy of the traffic signal permit and pictures of northbound traffic which consistently backs up through the Walker Blvd. intersection. Pedestrians attempting to cross at Walker would encounter walking between vehicles and vehicles exiting the Country Fair driveway. Snow on the sidewalks indicated that no peds were attempting to cross Peach St. at Walker.

**Pedestrian Traffic Signals (if applicable)**

- | Yes                                 | No                                  |   |
|-------------------------------------|-------------------------------------|---|
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Is there proper pedestrian timing established at the intersection? <u>38<sup>th</sup> / PEACH</u> |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | Is an all-pedestrian phase recommended in the study?  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Are the crosswalks in alignment with curb ramps?  |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Are pedestrian signals visible from the proposed crosswalk/curb ramp locations?                   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Are countdown pedestrian signals present?   |
| <input type="checkbox"/>            | <input checked="" type="checkbox"/> | Has the need for Accessible Pedestrian Signals (APS) been determined from the study?              |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Are pushbuttons proposed to be within the current ADA criteria?                                   |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Are all pushbutton locations accessible to all pedestrians?                                       |
| <input checked="" type="checkbox"/> | <input type="checkbox"/>            | Do all features satisfy applicable state and federal requirements?                                |

TE-672 (9-10)

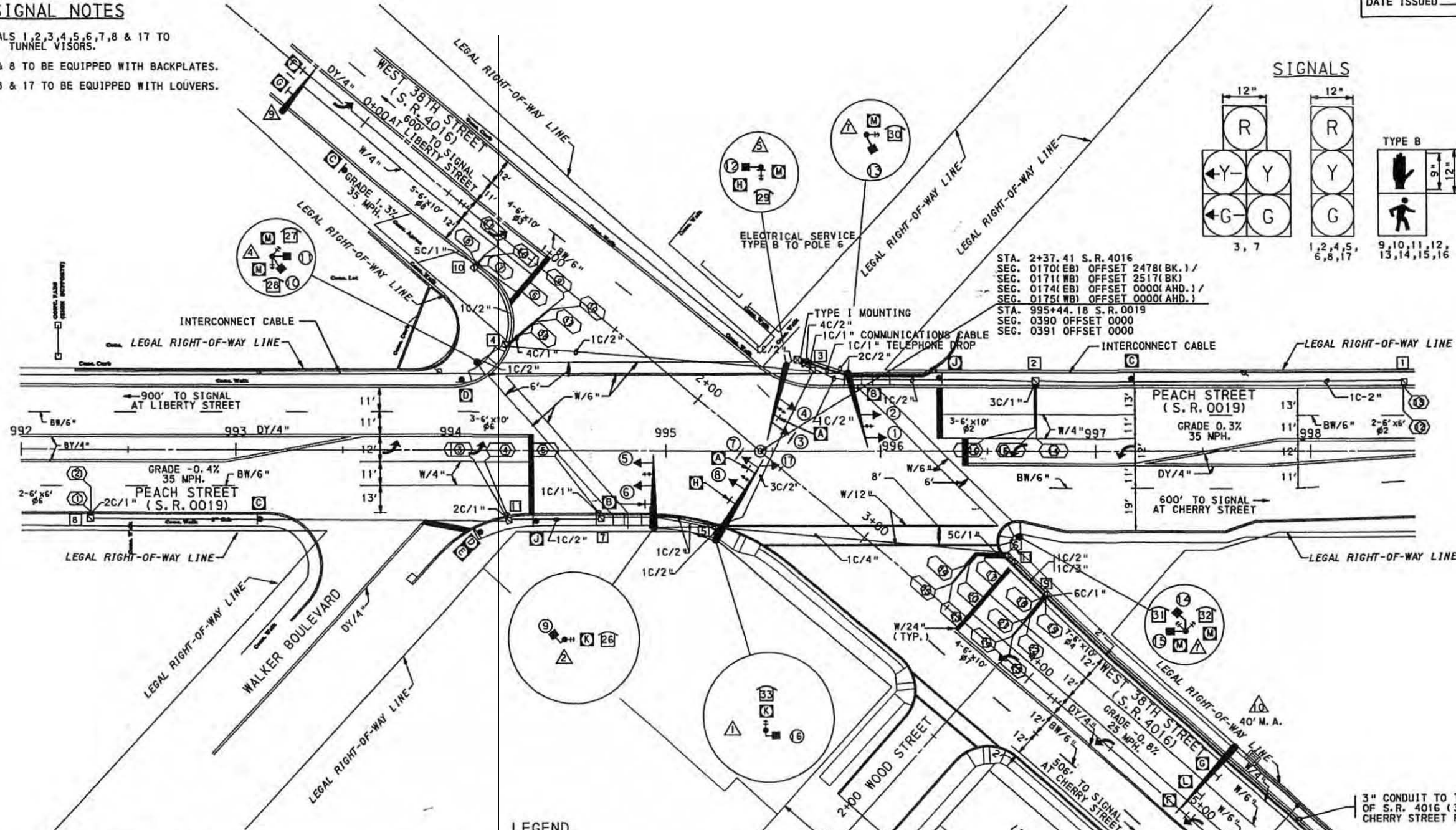
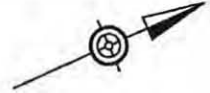
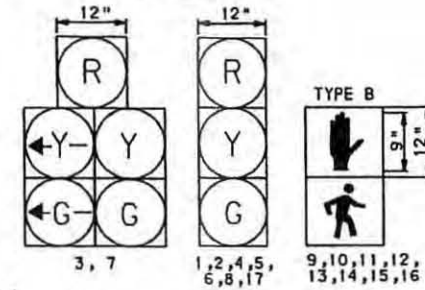
Recommendations/Justification			
The recommendation is to not place pedestrian facilities that cross Peach Street at Walker Boulevard. With the stacking of northbound Peach Street traffic, the potential crossing at Walker Blvd would create blind spots for pedestrians			
and also conflict with vehicles exiting Country Fair onto Peach Street. Adequate pedestrian facilities exist at the Peach Street / 38th Street crossing with countdown pedestrian signals.			
District Traffic Engineer Approval		Assistant District Executive Approval	
<i>Thomas J McClellan</i>	2/7/13	<i>Donald E. Hoff</i>	4/11/13
District Traffic Engineer	Date	District ADE of Design, Maintenance or Services	Date

PERMIT NO. 4235 SHEET 2 OF 3  
 DATE ISSUED 10-5-55 DATE REVISED 1-23-06

**SIGNAL NOTES**

1. VEHICULAR SIGNALS 1,2,3,4,5,6,7,8 & 17 TO BE EQUIPPED WITH TUNNEL VISORS.
2. SIGNALS 3,4,7 & 8 TO BE EQUIPPED WITH BACKPLATES.
3. SIGNALS 3,4,7,8 & 17 TO BE EQUIPPED WITH LOUVERS.

**SIGNALS**



**SIGN TABULATION**

SIGN	STANDARD	DESCRIPTION	SIZE	QUAN.
A	R10-12	LEFT TURN YIELD ON GREEN	30"x36"	2
B	D3-4	W 38th	60"x16"	2
C	R3-7L	LEFT LANE MUST TURN LEFT	30"x30"	3
D	R3-2	NO LEFT TURN	24"x24"	1
E	R1-1	STOP SIGN	30"x30"	1
F	R3-5L	LEFT TURN SIGN	30"x36"	2
G	R3-6SR	OPTIONAL RIGHT TURN SIGN	30"x36"	2
H	D3-4	PEACH	48"x16"	2
J	R10-11	NO TURN ON RED	24"x30"	2
K	R10-3B	EDUCATIONAL PEDESTRIAN SIGN	9"x12"	2
L	R3-5S	STRAIGHT-THROUGH SIGN	30"x36"	1
M	R10-4	PUSH BUTTON FOR WALK SIGNAL	9"x12"	6

**LEGEND**

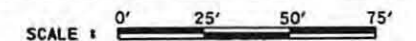
- ▲ - MAST ARM
- - PEDESTAL
- - VEHICULAR SIGNAL HEAD
- - PEDESTRIAN SIGNAL HEAD
- ⇄ - EMERGENCY VEHICLE PREEMPTION
- ▲ - POST MOUNTED SIGN
- ▲ - OVERHEAD MOUNTED SIGN
- ⊙ - VEHICLE DETECTOR
- ⇄ - PEDESTRIAN PUSH BUTTON/SIGN
- ⊞ - CONTROLLER ASSEMBLY
- ⊞ - JUNCTION BOX
- 1C/4" - CONDUIT/SIZE
- W/4" - SOLID WHITE LINE/WIDTH
- BW/4" - BROKEN WHITE LINE/WIDTH
- Y/4" - SOLID YELLOW LINE/WIDTH
- BY/4" - BROKEN YELLOW LINE/WIDTH
- DY/4" - DOUBLE SOLID YELLOW LINE/WIDTH

COUNTY : ERIE  
 MUNICIPALITY : CITY OF ERIE  
 INTERSECTION : WEST 38TH STREET (S.R. 4016) AND PEACH STREET (S.R. 0019)

APPROVED AND ACCEPTED:  
*Charles E. Dray* 1-23-06  
 MUNICIPAL OFFICIAL DATE

RECOMMENDED BY:  
*Randall A. Bond* 1-23-06  
 DISTRICT TRAFFIC ENGINEER DATE

D-469



SR 0019 and Walker Blvd



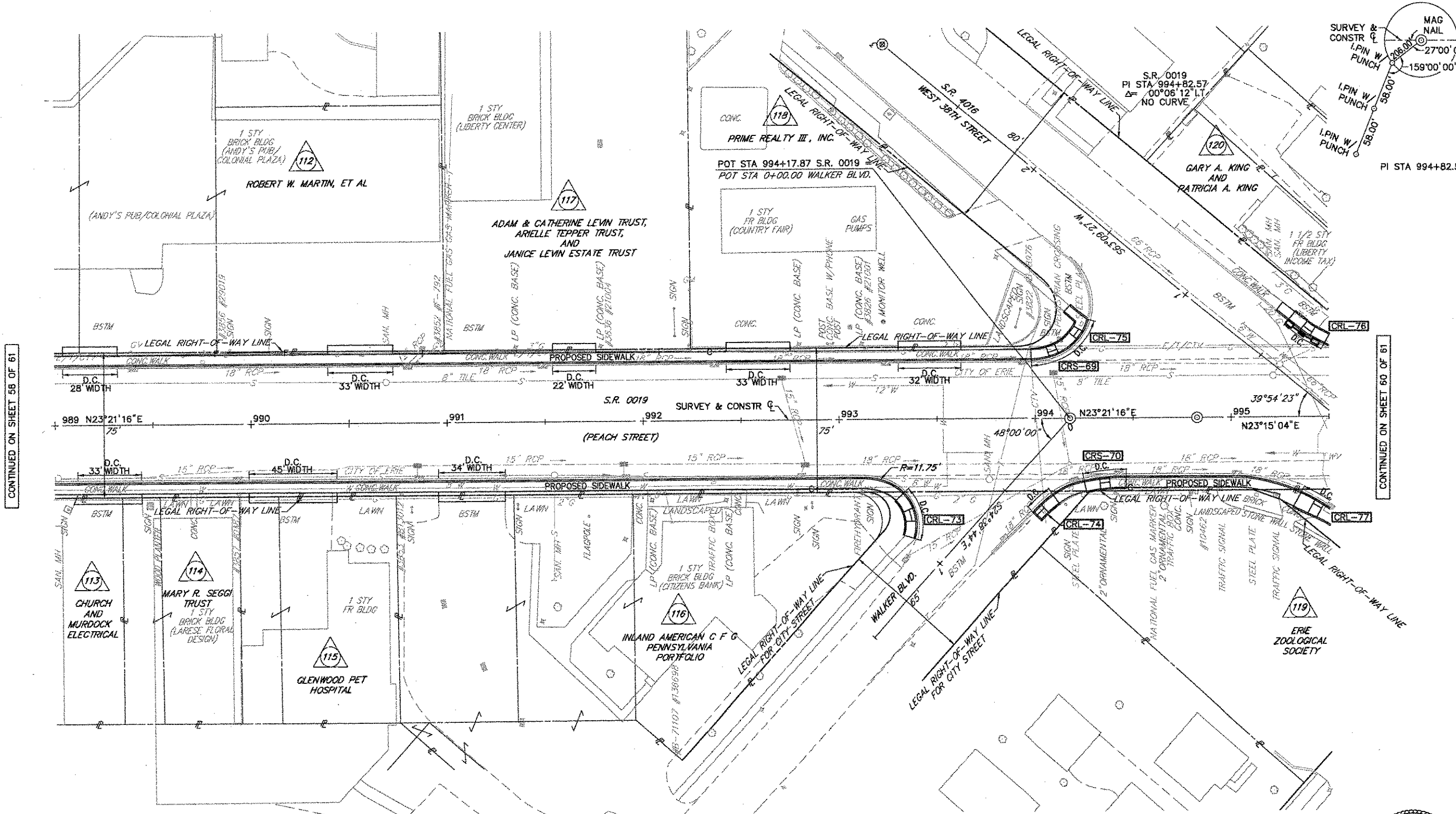
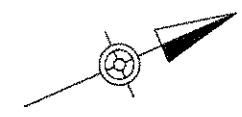








DISTRICT	COUNTY	ROUTE	SECTION	SHEET
1-0	ERIE	0019	A08	59 OF 61
CITY OF ERIE				
REVISION NUMBER	REVISIONS	DATE	BY	



CONTINUED ON SHEET 58 OF 61

CONTINUED ON SHEET 60 OF 61

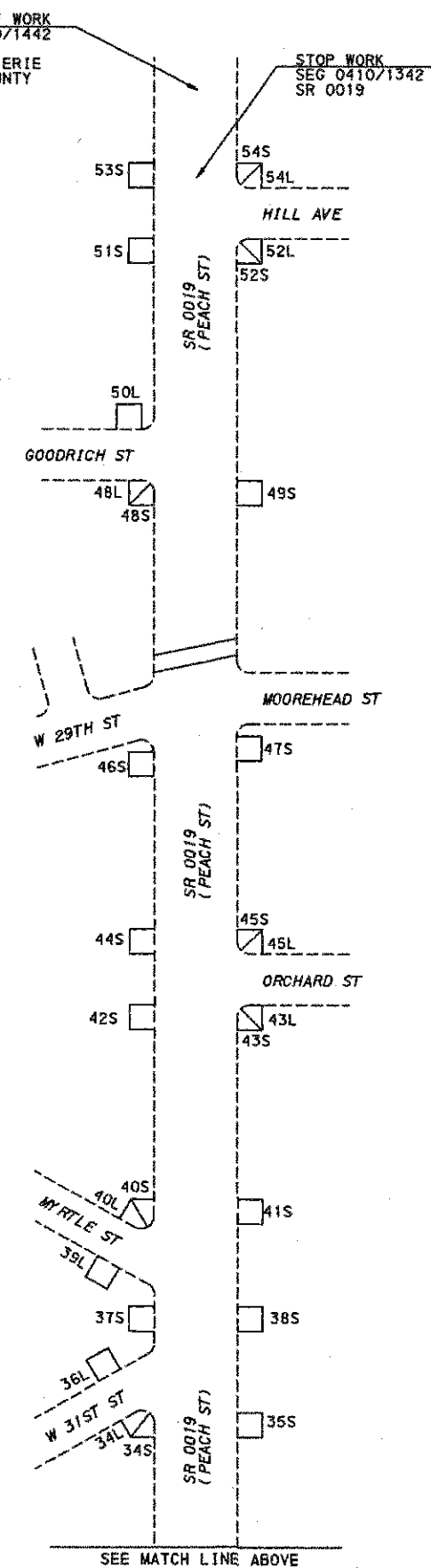
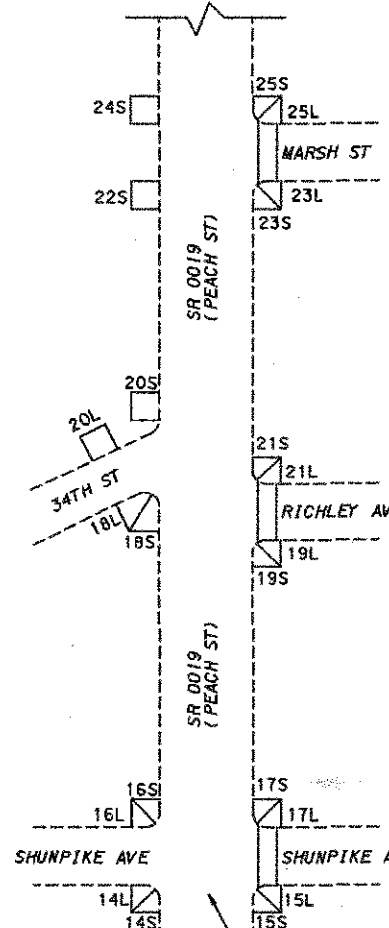
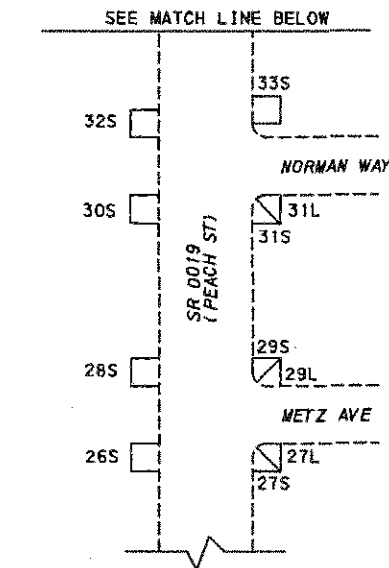
**LEGEND**  
 CRS-# CURB RAMP STATE  
 CRL-# CURB RAMP LOCAL



0 25 50  
 SCALE IN FEET

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
1-0	ERIE	0019	ADA	14 OF 16
CITY OF ERIE				
REVISION NUMBER	REVISIONS	DATE	BY	

CURB RAMP LOCATIONS



SR 0019  
(PEACH ST)

STATE LOCATIONS (S)  
35, 37, 38, 40-49, 51-54

LOCAL LOCATIONS (L)  
14-21, 23, 25, 27, 29, 31, 34, 36, 39, 40, 43, 45, 48, 50, 52, 54



NO SCALE

LEGEND

- 01S - STATE LOCATION NUMBER
- 01L - LOCAL LOCATION NUMBER
- - SINGLE TYPE RAMP
- ◻ - SHARED TYPE RAMPS
- ══ - EXISTING PAINTED CROSSWALK

**LIMIT OF WORK**  
SEG 0390/0954  
SEG 0391/0990  
SR 0019  
CITY OF ERIE  
ERIE COUNTY

**START WORK**  
SEG 0390/1054  
SEG 0391/1090  
SR 0019



OPERATOR: FILE NAME: G:\v-d8\er\er\_ada\_2011\er\er\_ada\_project\250005ADANEST.dgn

PLOTTED: 02-JUL-2012 15:01

0-8012 CADD (02-9D) REVISED (10-04)

### SR 20 (W 26<sup>th</sup> Street) and Hampton Ave



#93

**MOVEMENT, SEQUENCE & TIMING FOR SOLID STATE ACTUATED CONTROLLER**

PHASE	1				2				PREEMPTION OPERATION FOR PHASE 2		Emergency	Flashing
	Interval	1	2	3	4	5	6	7	8	9		
Signal	1-2	G	G	Y	R	R	R	R	R	R	R	Y
	3	G	G	Y	R	R	R	R	R	R	R	Y
	4	G	G	Y	R	R	R	R	R	R	R	Y
	5-6	R	R	R	R	G	G	Y	R	G	G	R
a	D	D	D	D	D	D	D	D	D	D	D	OFF
b	D	D	D	D	D	D	D	D	D	D	D	OFF
Fixed		30	15			30	15			30	15	
Minimum	-36-					7						
Passage						2						
Maximum						-25-						
Pedestrian	25	11				7	8					
Memory	MR					NL						

TIMING PLAN	CYCLE LENGTH (SECONDS)	FULL DEMAND PHASE SPLITS (%)		OFFSET (%)
		φ1	φ2	
I AVERAGE	70	67	33	0
II AM PEAK	85	76	24	0
III PM PEAK	70	59	41	0

**GENERAL NOTES**

INSTALLATION, OPERATION AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION AND OPERATION OF SIGNS, SIGNALS AND MARKINGS.

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION.

ALL MAINTENANCE WORK, INCLUDING TRIMMING TREES, NECESSARY FOR PROPER VISIBILITY OF THE SIGNS AND SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED ACCORDINGLY.

SUPPORT POLES FOR OVERHEAD SIGNALS SHALL HAVE A MINIMUM CLEARANCE OF 2 FEET BEHIND THE FACE OF THE CURB OR 10 FEET FROM THE EDGE OF THE TRAVELED ROADWAY.

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A CLEARANCE OF 17 FEET ABOVE THE ROADWAY.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS, MEASURED AT RIGHT ANGLES TO THE APPROACH, SHALL BE 8 FEET.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF ACT 172, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, EFFECTIVE DATE JUNE 10, 1987. PRIOR TO CONSTRUCTION CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

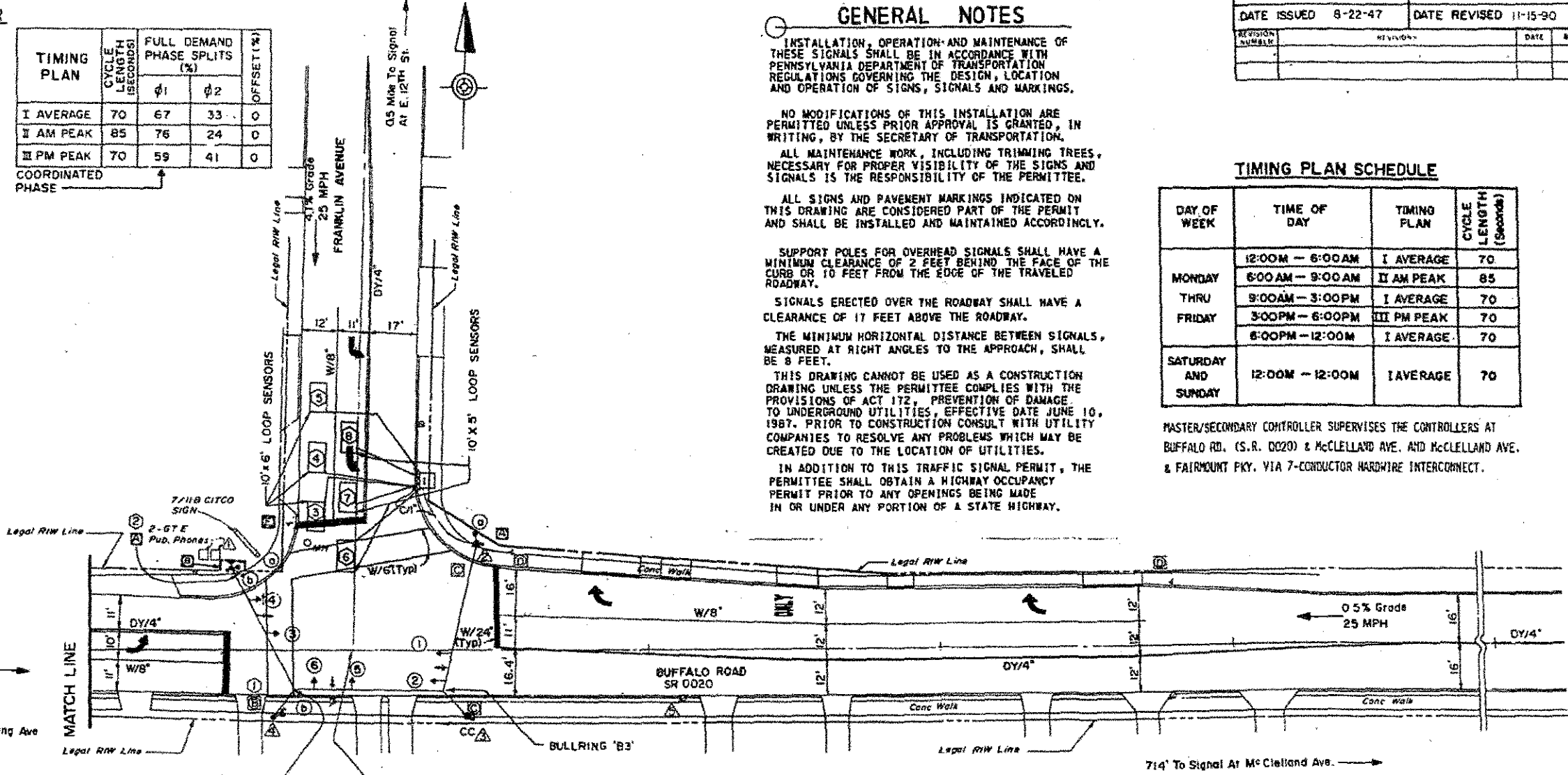
IN ADDITION TO THIS TRAFFIC SIGNAL PERMIT, THE PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT PRIOR TO ANY OPENINGS BEING MADE IN OR UNDER ANY PORTION OF A STATE HIGHWAY.

PERMIT No. 1389	SHEET 2 of 2
DATE ISSUED 8-22-47	DATE REVISED 11-15-90
REVISION NUMBER	REVISION DATE BY

**TIMING PLAN SCHEDULE**

DAY OF WEEK	TIME OF DAY	TIMING PLAN	CYCLE LENGTH (Seconds)
MONDAY THRU FRIDAY	12:00M - 6:00AM	I AVERAGE	70
	6:00AM - 9:00AM	II AM PEAK	85
SATURDAY AND SUNDAY	9:00AM - 3:00PM	I AVERAGE	70
	3:00PM - 6:00PM	III PM PEAK	70
	6:00PM - 12:00M	I AVERAGE	70

MASTER/SECONDARY CONTROLLER SUPERVISES THE CONTROLLERS AT BUFFALO RD. (S.R. 0020) & McCLELLAND AVE. AND McCLELLAND AVE. & FAIRMOUNT PKY. VIA 7-CONDUCTOR HARDWARE INTERCONNECT.



Controller to be coordinated with signal at Buffalo Road and McClelland Ave

● Upon Pedestrian Actuation Only

-0.5% Grade 25 MPH

2702' To Signal At Downing Ave

**SIGNS**

Plan Symbol	Description	Size W x H	Qty.
(Symbol)	R10-4L Push Button For Walk Signal	9 X 12	2
(Symbol)	R10-4R Push Button For Walk Signal	9 X 12	2
(Symbol)	R9-3 No Pedestrian Crossing	18 X 18	2
(Symbol)	R3-7R Right Lane Must Turn Right	30 X 30	2
(Symbol)	R3-7L Left Lane Must Turn Left	30 X 30	2

**EMERGENCY PREEMPTION NOTE**

PROVIDE EMERGENCY VEHICLE PREEMPTION EQUIPMENT IN THE CONTROLLER CABINET. UPON PREEMPTION UTILIZE THE CONTROLLER'S INTERNAL PREEMPTION SEQUENCES TO PROVIDE THE FOLLOWING:

A. A YELLOW CLEARANCE INTERVAL FOR ALL CONFLICTING GREEN INDICATIONS FOLLOWED BY AN ALL RED INTERVAL.

B. A FLASHING "DON'T WALK" INTERVAL FOR ANY "WALK" INDICATION.

C. TIME OUT OF ANY FLASHING "DON'T WALK" INTERVAL.

D. TIME OUT OF ANY YELLOW CLEARANCE INTERVAL.

E. TIME OUT OF ANY ALL RED CLEARANCE.

UTILIZE CONTROLLER TIMINGS FOR INTERVALS BEING CLEARED.

UTILIZE THE FOLLOWING PREEMPTION SEQUENCE: FIRST, A FLASHING "DON'T WALK" FOR ANY "WALK" INDICATION. NEXT, A YELLOW CLEARANCE INTERVAL IF A CONFLICTING PHASE IS CALLED. THEN, AN ALL RED INTERVAL IF PRECEDED BY A YELLOW CLEARANCE. THEN, A GREEN INDICATION ON THE SELECTED STREET WITH A RED INDICATION GIVEN TO ALL OTHER STREETS AND A STEADY "DON'T WALK" INDICATION ON ALL PEDESTRIAN SIGNALS FOR THE DURATION OF THE PREEMPTION.

UPON TERMINATION OF PREEMPTION, SIGNAL IS TO RETURN TO NORMAL OPERATION.

**LEGEND**

- (Symbol) - Mast Arm
- (Symbol) - Strain Pole
- (Symbol) - Pedestal
- (Symbol) - Vehicular Signal Head
- (Symbol) - Pedestrian Signal Head
- (Symbol) - Sign
- (Symbol) - Vehicle Detector
- (Symbol) - Junction Box
- (Symbol) - Pedestrian Push Button/Sign
- CC (Symbol) - Controller Assembly
- W/4" - Solid White Line/Width
- BW/4" - Broken White Line/Width
- Y/4" - Solid Yellow Line/Width
- BY/4" - Broken Yellow Line/Width
- DY/4" - Double Solid Yellow Line/Width
- (Symbol) - Existing Utility Pole
- (Symbol) - Emergency Vehicle Preempt Confirmation Light
- C/2" - Conduit / Size

County: ERIE

Municipality: CITY OF ERIE

Intersection: BUFFALO ROAD (S.R. 0020) & FRANKLIN AVENUE

Reviewed: [Signature] 7/1/08  
Municipal Official Date

Recommended: [Signature] 7-1-08  
District Traffic Engineer Date

Scale: 20 0 20 40

D-728

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
1-0	ERIE		000	12 OF 13
CITY OF ERIE				
REVISION NUMBER	REVISIONS	DATE	BY	

BM-2 ELEV. 761.75'  
S.R. 0505 SURVEY BASELINE  
19' LT. STA. 1066+60  
NAIL IN POLE #E4626

BM-1 ELEV. 765.34'  
NORMAN WAY SURVEY BASELINE  
15' LT. STA. 2+00  
NAIL IN POLE

**EXISTING STRUCTURE DATA**  
STA: 0+54.34  
TYPE: STEEL STRINGER  
31'-1 1/2" OUT TO OUT  
SPAN: 50'-0" CLEAR OPENING  
CLEAR ROADWAY WIDTH: 22'-6"  
EXISTING WATERWAY OPENING: 371.2 SF

**PROPOSED STRUCTURE DATA**  
STA: 0+54.34  
TYPE: SPREAD 48" X 24" P/S BOX BEAM  
SPAN: 51'-6"  
SKEW: 90°00'00"  
ROADWAY WIDTH: 26'-0" CURB TO CURB  
STRUCTURE NO.: 25-2829  
RECOMMENDED:  
PROPOSED WATERWAY OPENING: 378.5 SF

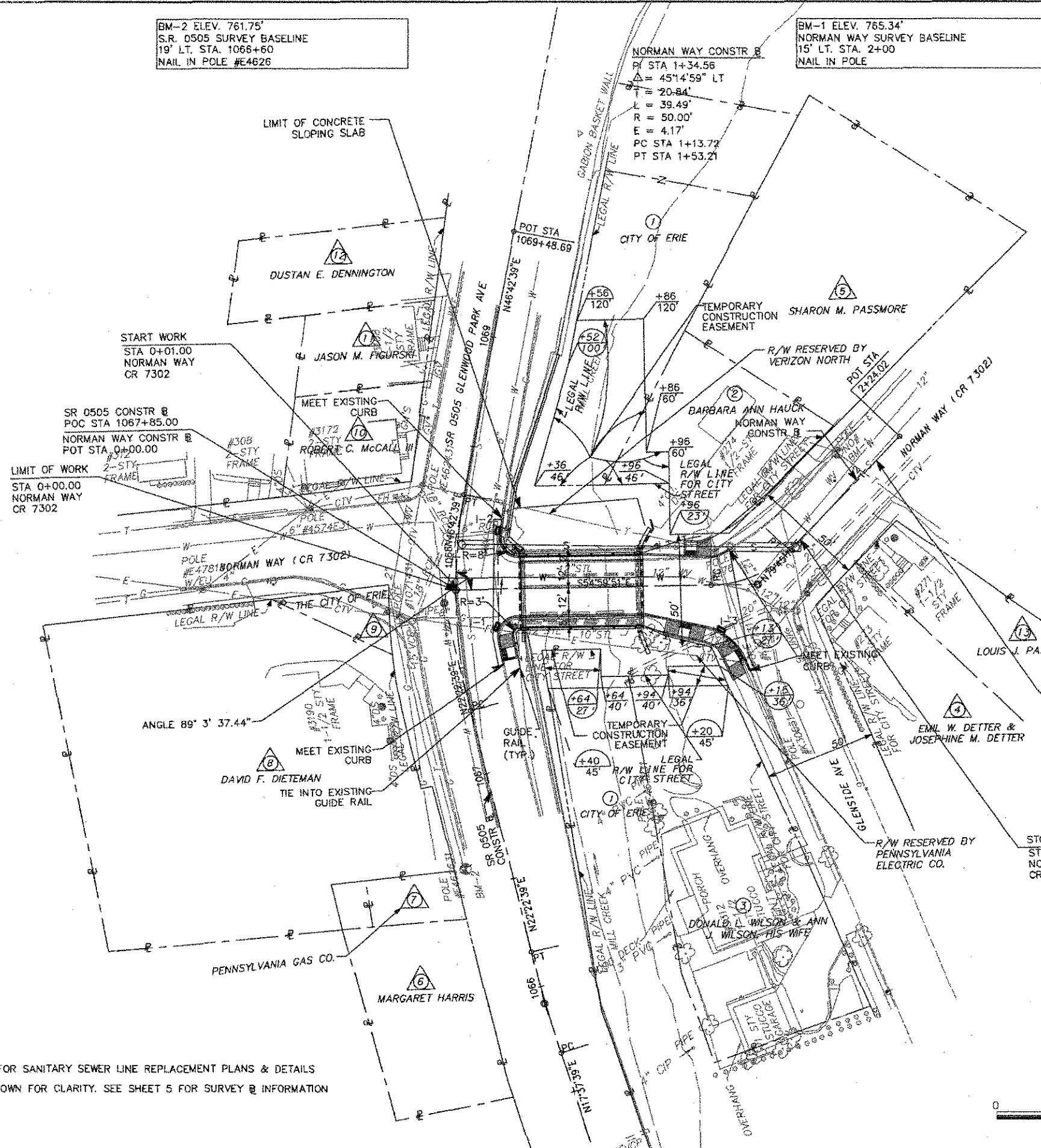
**HYDRAULIC DATA**  
DRAINAGE AREA = 9.52 SQ. MI.  
DESIGN FLOOD:  
FREQUENCY = 10 YEARS  
MAGNITUDE = 1508.6 CFS  
VELOCITY = 6.70 FPS  
ELEV = 756.05  
100 YEAR FLOOD RISK ASSESSMENT:  
MAGNITUDE = 2421.5 CFS  
VELOCITY = 7.90 FPS  
ELEV = 757.68

**INLET DESIGNATION TABLE**

I-1 CITY OF ERIE INLET	RIM ELEV: 760.82	INV N: 755.82
I-2 CITY OF ERIE INLET	RIM ELEV: 760.32	INV E: 754.44 INV W: 754.44 INV S: 754.44
I-3 CITY OF ERIE INLET	RIM ELEV: 762.80	INV E: 756.52 INV W: 756.52

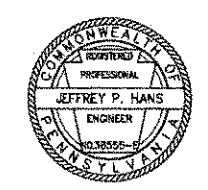
SR 0505 CONSTR B  
PI STA 1067+80.12  
Δ = 24°20'00" RT  
T = 49.81'  
L = 98.11'  
R = 231.01'  
PC STA 1067+30.31  
PT STA 1068+28.42  
NO SUPERELEVATION

SR 0505 CONSTR B  
PI STA 1065+92.15  
Δ = 4°45'00" RT  
T = 23.79'  
L = 47.56'  
R = 537.69'  
PC STA 1065+68.36  
PT STA 1066+15.92  
NO SUPERELEVATION



**NOTE:**  
1. SEE ALSO PLANS FOR SANITARY SEWER LINE REPLACEMENT PLANS & DETAILS  
2. SURVEY @ NOT SHOWN FOR CLARITY. SEE SHEET 5 FOR SURVEY @ INFORMATION

0 25 50 FEET



FOR PROFILE SEE SHEET 13 SURVEY BOOK # 2310-1

OPERATOR: Sussan  
 FILE NAME: 11design11a11.dwg  
 PLOTTED: 9-8022 CAD (09-09) REVISED (10-04)



### SR 4016 (West 38<sup>th</sup>) and Myrtle



### SR 4016 (West 38<sup>th</sup>) and Eliot Road



TE-672 (9-10)

# PEDESTRIAN ACCOMMODATION AT INTERSECTIONS CHECKLIST



# #139

PLEASE TYPE OR PRINT ALL INFORMATION IN BLUE OR BLACK INK

As specified in the MUTCD, Section 4E, an engineering study shall be conducted to determine the need for pedestrian accommodation at signalized intersections and the related design and operational features. Based on the engineering study and engineering judgment, proper documentation shall be made at all new signalized intersections and modifications to existing signalized intersections. This documentation shall be provided with guidance from this checklist.

When pedestrian accommodations will not be provided at an intersection, signalized or not, proper justification must be documented. Below is a checklist of information that may be relevant in the determination of pedestrian needs and warranted accommodations at an intersection. Not all of the information below is required to make a determination. This form can be used to summarize the needs and accommodations of a corridor or a single intersection.

Intersection Background Information			
DATE December 21, 2012	DISTRICT 1-0	COUNTY Erie	
MUNICIPALITY City of Erie		INTERSECTION SR 4016 & Wayne	
SUBMITTED BY Paul A. Miller, P.E.		STREET ADDRESS 255 Elm Street	
CITY Oil City	STATE PA	ZIP 16301	TELEPHONE NUMBER 814-678-7107

### Project Overview

Provide a Project Description and Scope of the Project.  
 Bike/Ped Checklist Completed.  Yes  No Curb ramp only project to update missing and non-compliant ramps

### Existing Facility Description

Yes	No	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pedestrian facilities present (i.e., sidewalks, curb ramps, crosswalks, pedestrian signals, etc.)?
<input type="checkbox"/>	<input type="checkbox"/>	Are there descriptions of each quadrant of the existing intersection (photos are strongly recommended)?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are near-by land uses documented?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pedestrian facilities near the intersection (i.e., sidewalks, bus stops, trails, etc.)?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is there current evidence of pedestrians using the intersection (worn paths, observed activity)?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Were special accommodations made in the past for pedestrians at the intersection?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are restrictions for pedestrians present? (No Ped signs, limited crossings)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is this a defined walking route or safe route to school route?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are physical restrictions or right-of-way restrictions present?
<input type="checkbox"/>	<input type="checkbox"/>	If pedestrian signals are present, is proper traffic signal timing designated for pedestrians at the intersection?

### Proposed Facility Description

Yes	No	
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Will the proposed improvements generate new or additional pedestrian traffic?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Does the proposed facility introduce possible additional restrictions for pedestrians?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are new or existing pedestrian signals proposed at the intersection?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are sidewalks proposed as part of the project?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are detailed descriptions of changes to each quadrant documented?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Do near-by land uses change as part of the project?
<input type="checkbox"/>	<input type="checkbox"/>	Are additional pedestrian facilities proposed for the intersection?

### Outreach Efforts

Has contact and discussion concerning pedestrian accommodations at the intersection been made with the following?

Yes	No	
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Municipality (s)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Transit Organization (s)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	School District (s)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Meeting (s)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Emergency Services
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Advocacy Groups
<input type="checkbox"/>	<input type="checkbox"/>	Other (s) _____

TE-672 (9-10)

**Intersection Details**

(The information below may be useful in the determination of pedestrian needs and warranted accommodations at uncontrolled intersections. Not all of the information is required to make a determination. The minor roadway information may also be needed in some situations.)

Roadway classification: Urban Principal Arterial  
 Roadway Typology: Suburban Center  
 Speed Limit (MPH): 35  
 Design Speed: 35  
 ADT: 12,941  
 Percentage of Trucks: 3  
 Travel lanes: 4 with left turn lane

Is there a sight distance deficiency?  Yes  No

If yes, explain. \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

Sidewalk:  Yes  No      Shoulders:  Yes  No      Curb:  Yes  No

Is parking permitted on the roadway?  Yes  No

Municipal recommendation: \_\_\_\_\_  
 \_\_\_\_\_

Pedestrian generators? CVS, Arby's

Distance to next available crossing: 70'

Was a crash analysis completed?  Yes  No

Are there pedestrian crashes?  Yes  No

Has a pedestrian gap analysis been performed at the location?  Yes  No

[http://ftp.dot.state.pa.us/transfer/Traffic Signals/Unsignalized Intersection Ped Calcs from HCS.xlsx](http://ftp.dot.state.pa.us/transfer/Traffic%20Signals/Unsignalized%20Intersection%20Ped%20Calcs%20from%20HCS.xlsx)

Are there other geometric concerns? If yes, explain: The intersection of Wayne Street is approximately 70' East from the signalized intersection of East 38th / Pine Avenue. Attached is a copy of the signal permit and a picture of traffic which consistently backs up to Wayne Street during all hours of the day. There is only room for 2 vehicles and creating another 38th Street crossing at Wayne would not be recommended or in the interest of safety. Proper pedestrian facilities exist at East 38th / Pine Avenue with DWS and pedestrian push button poles.

**Pedestrian Traffic Signals (if applicable)**

Yes	No	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Is there proper pedestrian timing established at the intersection? <span style="float: right;">③ 38th Street &amp; Pine</span>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is an all-pedestrian phase recommended in the study?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are the crosswalks in alignment with curb ramps?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pedestrian signals visible from the proposed crosswalk/curb ramp locations?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are countdown pedestrian signals present?
<input type="checkbox"/>	<input type="checkbox"/>	Has the need for Accessible Pedestrian Signals (APS) been determined from the study?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are pushbuttons proposed to be within the current ADA criteria?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Are all pushbutton locations accessible to all pedestrians?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	Do all features satisfy applicable state and federal requirements?

TE-672 (9-10)

**Recommendations/Justification**

The recommendation is to not place pedestrian facilities that cross East 38th Street at Wayne street due to close proximity of existing pedestrian facilities at East 38th Street and Pine Avenue. There are currently no pedestrian accidents at either intersection and the stacking of Westbound vehicles at Pine Street would hinder the pedestrian movements and possibly lead to an unsafe condition with pedestrians mixing in between the traffic. There are benches for the bus stop which could be moved to allow more room for pedestrian movements at East 38th / Pine Avenue.

**District Traffic Engineer Approval**

**Assistant District Executive Approval**

*Thomas J McClintock*

*1/9/13*

*Donna E Hall*

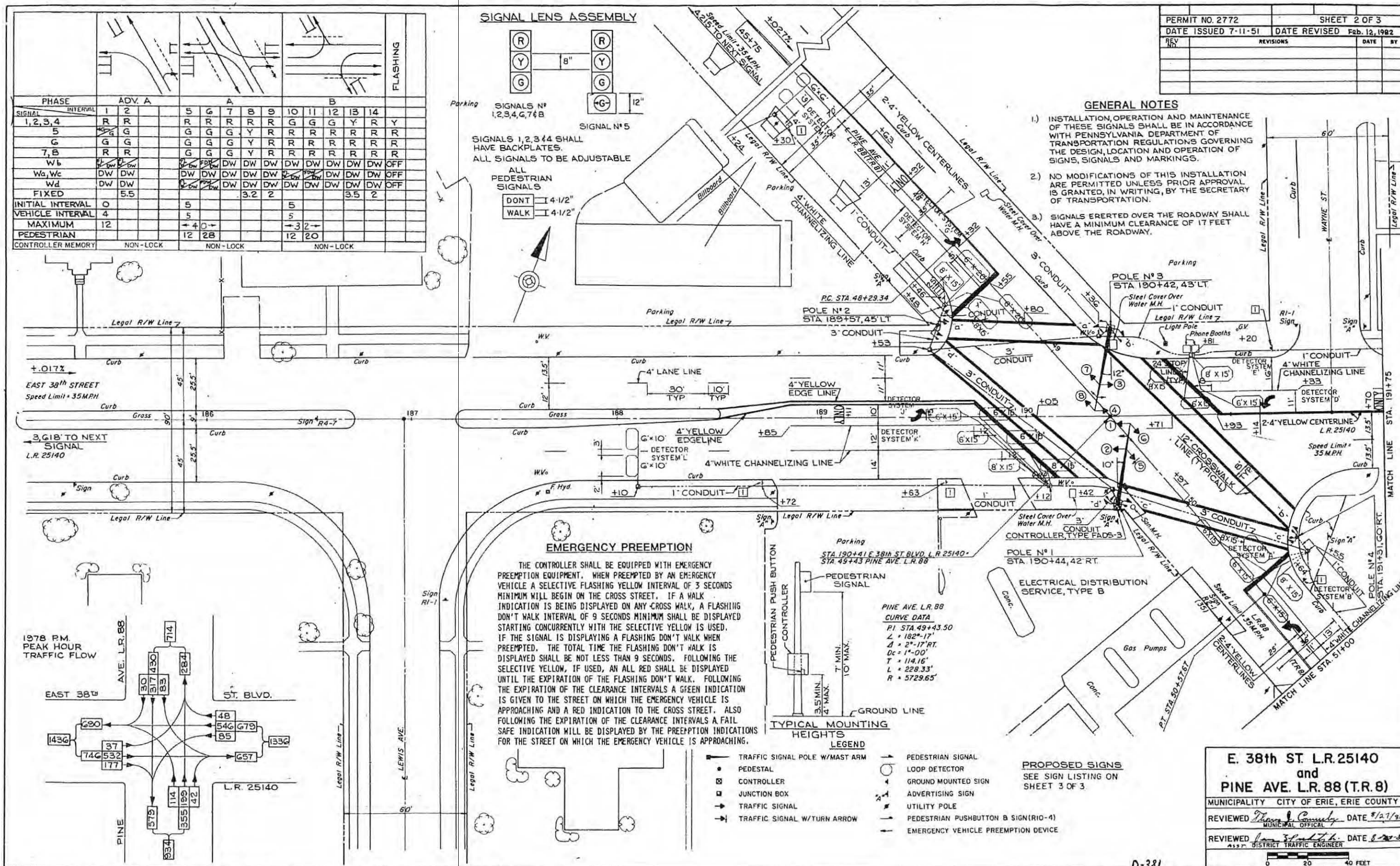
*1/9/13*

District Traffic Engineer

Date

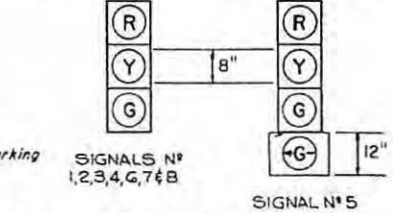
District ADE of Design, Maintenance or Services

Date



PHASE	INTERVAL	ADV. A		A						B				FLASHING
		1	2	5	6	7	8	9	10	11	12	13	14	
1,2,3,4	R	R		R	R	R	R	R	R	R	R	R	R	Y
5	G	G		G	G	G	Y	R	R	R	R	R	R	R
6	G	G		G	G	G	Y	R	R	R	R	R	R	R
7,B	R	R		G	G	G	Y	R	R	R	R	R	R	R
Wb	DW	DW		DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	OFF
Wa,Wc	DW	DW		DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	OFF
Wd	DW	DW		DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	OFF
FIXED		5.5				3.2	2				3.5	2		
INITIAL INTERVAL	0			5					5					
VEHICLE INTERVAL	4			5					5					
MAXIMUM	12			4	0				3	2				
PEDESTRIAN				12	28				12	20				
CONTROLLER MEMORY		NON-LOCK		NON-LOCK					NON-LOCK					

**SIGNAL LENS ASSEMBLY**



SIGNALS 1, 2, 3, 4, 6, 7, 8, 9 SHALL HAVE BACKPLATES.  
ALL SIGNALS TO BE ADJUSTABLE

ALL PEDESTRIAN SIGNALS  
DONT 4-1/2"  
WALK 4-1/2"

PERMIT NO. 2772	SHEET 2 OF 3		
DATE ISSUED 7-11-51	DATE REVISED Feb. 12, 1982		
REV. NO.	REVISIONS	DATE	BY

**GENERAL NOTES**

- 1) INSTALLATION, OPERATION AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION AND OPERATION OF SIGNS, SIGNALS AND MARKINGS.
- 2) NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION.
- 3) SIGNALS ERRECTED OVER THE ROADWAY SHALL HAVE A MINIMUM CLEARANCE OF 17 FEET ABOVE THE ROADWAY.

**EMERGENCY PREEMPTION**

THE CONTROLLER SHALL BE EQUIPPED WITH EMERGENCY PREEMPTION EQUIPMENT. WHEN PREEMPTED BY AN EMERGENCY VEHICLE A SELECTIVE FLASHING YELLOW INTERVAL OF 3 SECONDS MINIMUM WILL BEGIN ON THE CROSS STREET. IF A WALK INDICATION IS BEING DISPLAYED ON ANY CROSS WALK, A FLASHING DON'T WALK INTERVAL OF 9 SECONDS MINIMUM SHALL BE DISPLAYED STARTING CONCURRENTLY WITH THE SELECTIVE YELLOW IS USED. IF THE SIGNAL IS DISPLAYING A FLASHING DON'T WALK WHEN PREEMPTED, THE TOTAL TIME THE FLASHING DON'T WALK IS DISPLAYED SHALL BE NOT LESS THAN 9 SECONDS. FOLLOWING THE SELECTIVE YELLOW, IF USED, AN ALL RED SHALL BE DISPLAYED UNTIL THE EXPIRATION OF THE FLASHING DON'T WALK. FOLLOWING THE EXPIRATION OF THE CLEARANCE INTERVALS A GREEN INDICATION IS GIVEN TO THE STREET ON WHICH THE EMERGENCY VEHICLE IS APPROACHING AND A RED INDICATION TO THE CROSS STREET. ALSO FOLLOWING THE EXPIRATION OF THE CLEARANCE INTERVALS A FAIL SAFE INDICATION WILL BE DISPLAYED BY THE PREEMPTION INDICATIONS FOR THE STREET ON WHICH THE EMERGENCY VEHICLE IS APPROACHING.

- TYPICAL MOUNTING HEIGHTS LEGEND**
- TRAFFIC SIGNAL POLE W/MAST ARM
  - PEDESTAL
  - CONTROLLER
  - JUNCTION BOX
  - TRAFFIC SIGNAL
  - TRAFFIC SIGNAL W/TURN ARROW
  - PEDESTRIAN SIGNAL
  - LOOP DETECTOR
  - GROUND MOUNTED SIGN
  - ADVERTISING SIGN
  - UTILITY POLE
  - PEDESTRIAN PUSHBUTTON B SIGN(R10-4)
  - EMERGENCY VEHICLE PREEMPTION DEVICE

**PROPOSED SIGNS**  
SEE SIGN LISTING ON SHEET 3 OF 3

**E. 38th ST. L.R. 25140 and PINE AVE. L.R. 88 (T.R. 8)**

MUNICIPALITY CITY OF ERIE, ERIE COUNTY

REVIEWED *Thomas J. Conroy* DATE 8/27/91  
MUNICIPAL OFFICIAL

REVIEWED *John J. Conroy* DATE 8/27/91  
DISTRICT TRAFFIC ENGINEER

D-381

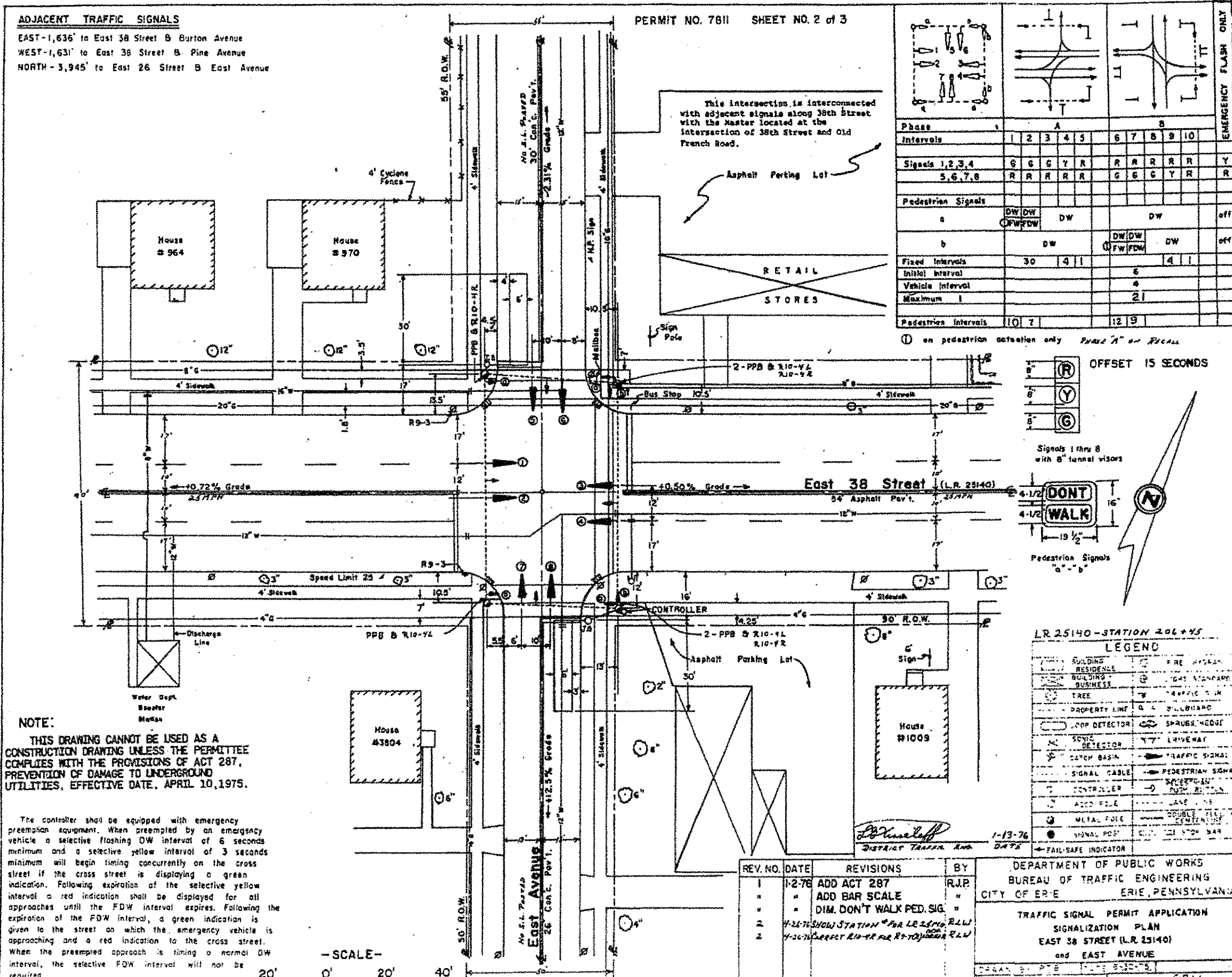
0 20 40 FEET

SR 4016 (East 38th) and Wayne Street

#139

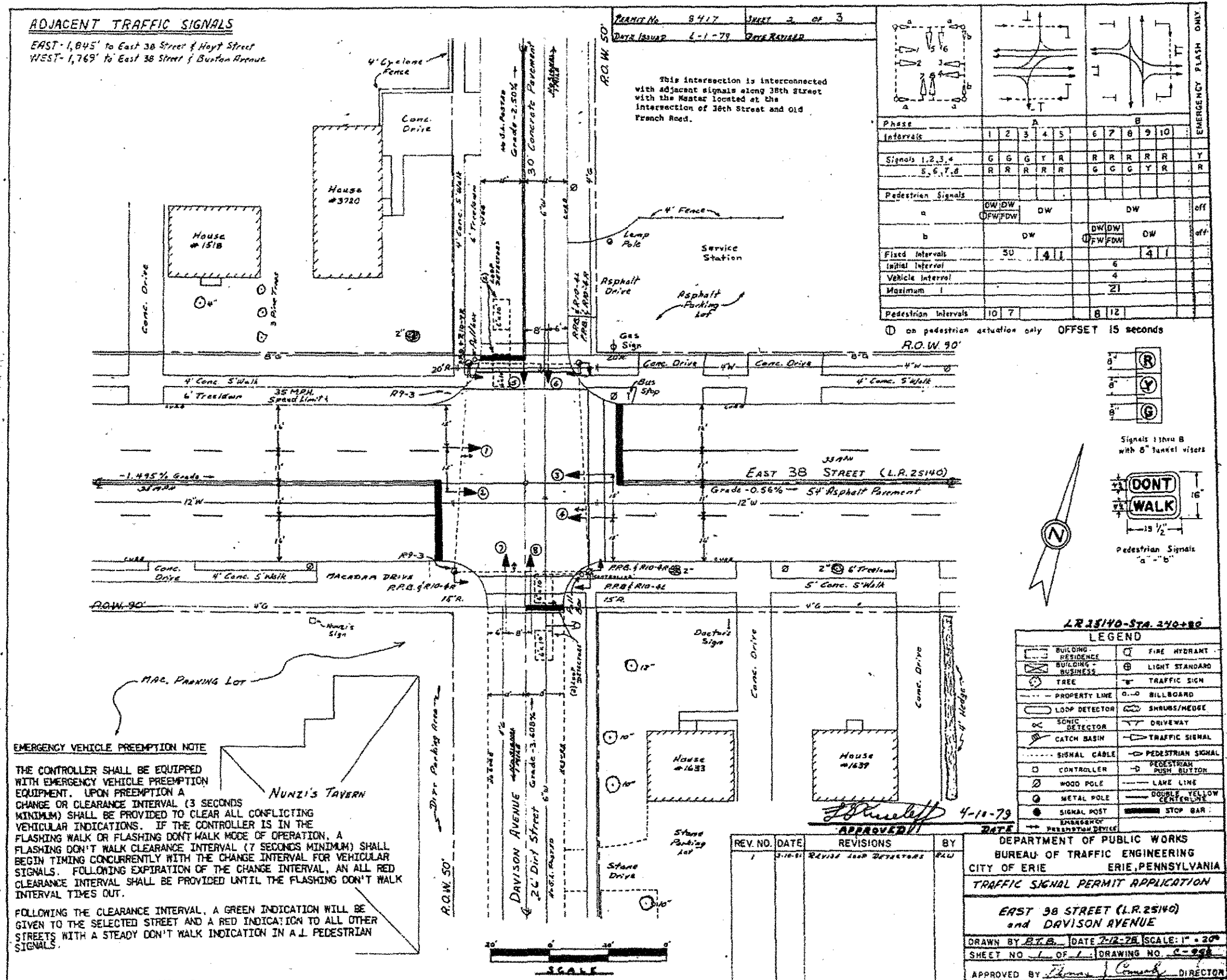


#143





#153

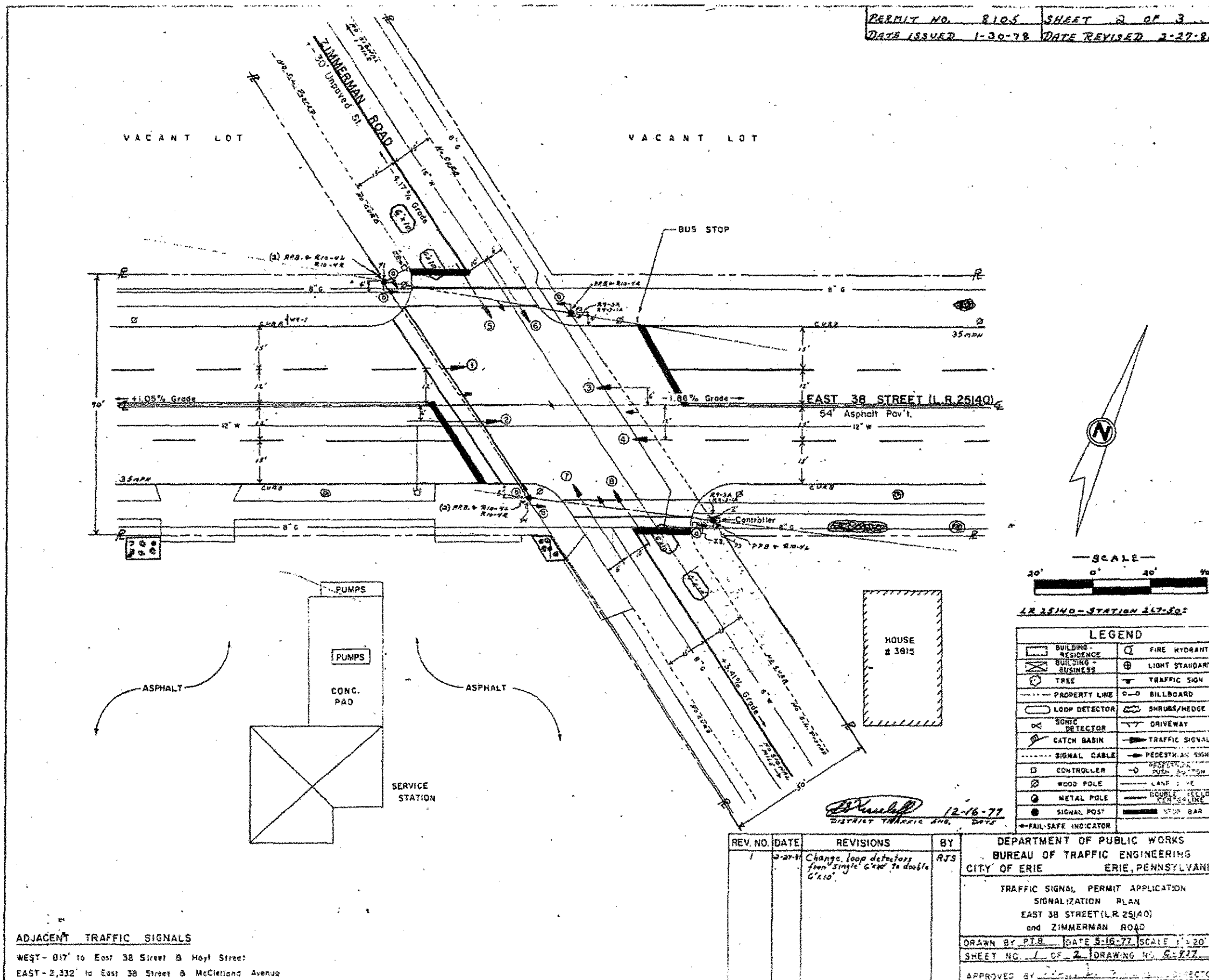


**EMERGENCY VEHICLE PREEMPTION NOTE**

THE CONTROLLER SHALL BE EQUIPPED WITH EMERGENCY VEHICLE PREEMPTION EQUIPMENT. UPON PREEMPTION A CHANGE OR CLEARANCE INTERVAL (3 SECONDS MINIMUM) SHALL BE PROVIDED TO CLEAR ALL CONFLICTING VEHICULAR INDICATIONS. IF THE CONTROLLER IS IN THE FLASHING WALK OR FLASHING DON'T WALK MODE OF OPERATION, A FLASHING DON'T WALK CLEARANCE INTERVAL (7 SECONDS MINIMUM) SHALL BEGIN TIMING CONCURRENTLY WITH THE CHANGE INTERVAL FOR VEHICULAR SIGNALS. FOLLOWING EXPIRATION OF THE CHANGE INTERVAL, AN ALL RED CLEARANCE INTERVAL SHALL BE PROVIDED UNTIL THE FLASHING DON'T WALK INTERVAL TIMES OUT.

FOLLOWING THE CLEARANCE INTERVAL, A GREEN INDICATION WILL BE GIVEN TO THE SELECTED STREET AND A RED INDICATION TO ALL OTHER STREETS WITH A STEADY DON'T WALK INDICATION IN ALL PEDESTRIAN SIGNALS.

#158

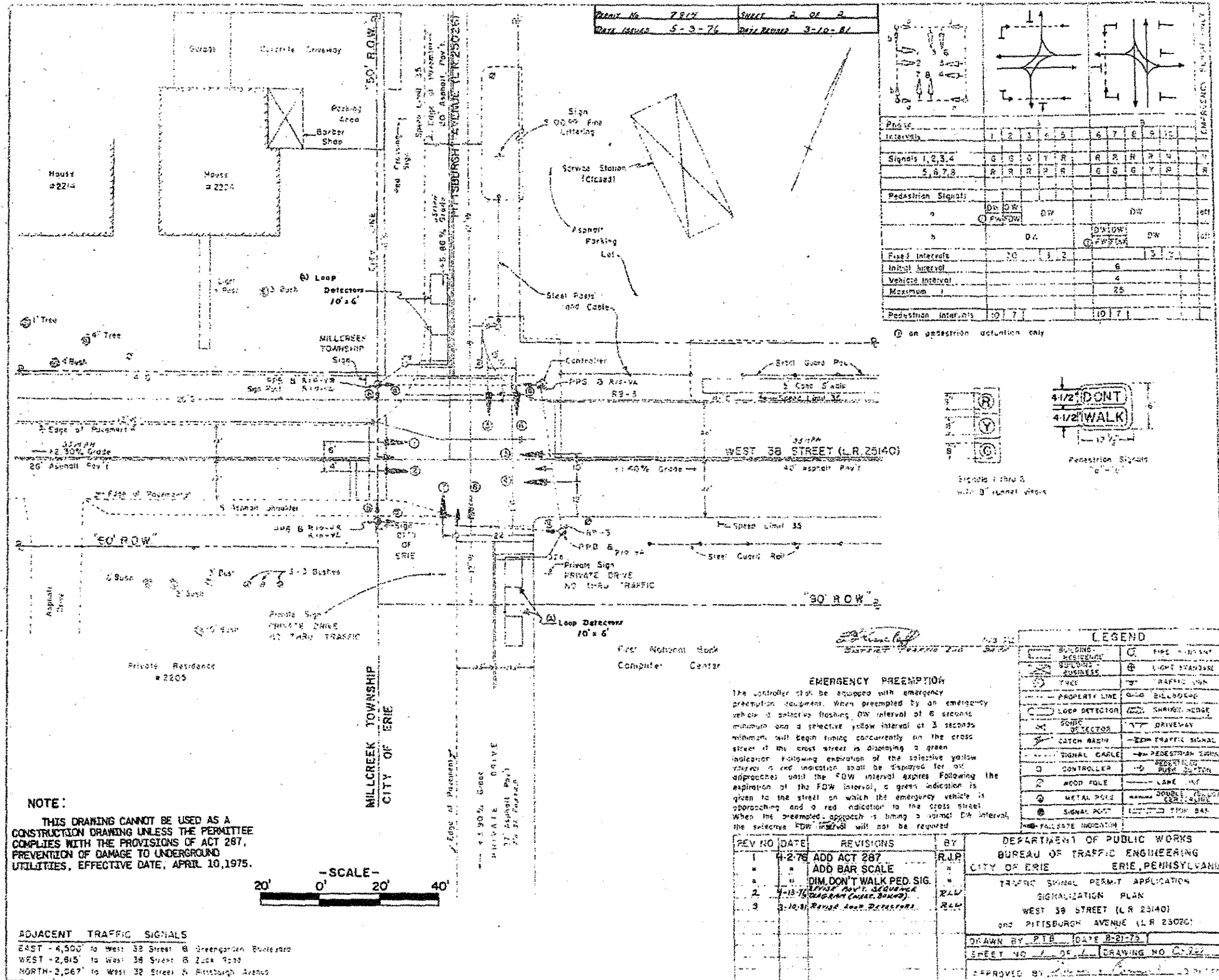


ADJACENT TRAFFIC SIGNALS  
WEST - 017' to East 38 Street & Hoyt Street  
EAST - 2,332' to East 38 Street & McClelland Avenue

SR 4016 (E 38<sup>th</sup>) and Rice



#164



SR 4017 (Pittsburgh Ave) and 22<sup>nd</sup> Street



### SR 4017 (Pittsburgh Ave) and 21<sup>st</sup> Street



### SR 4017 (Pittsburgh Ave) and Manor Drive

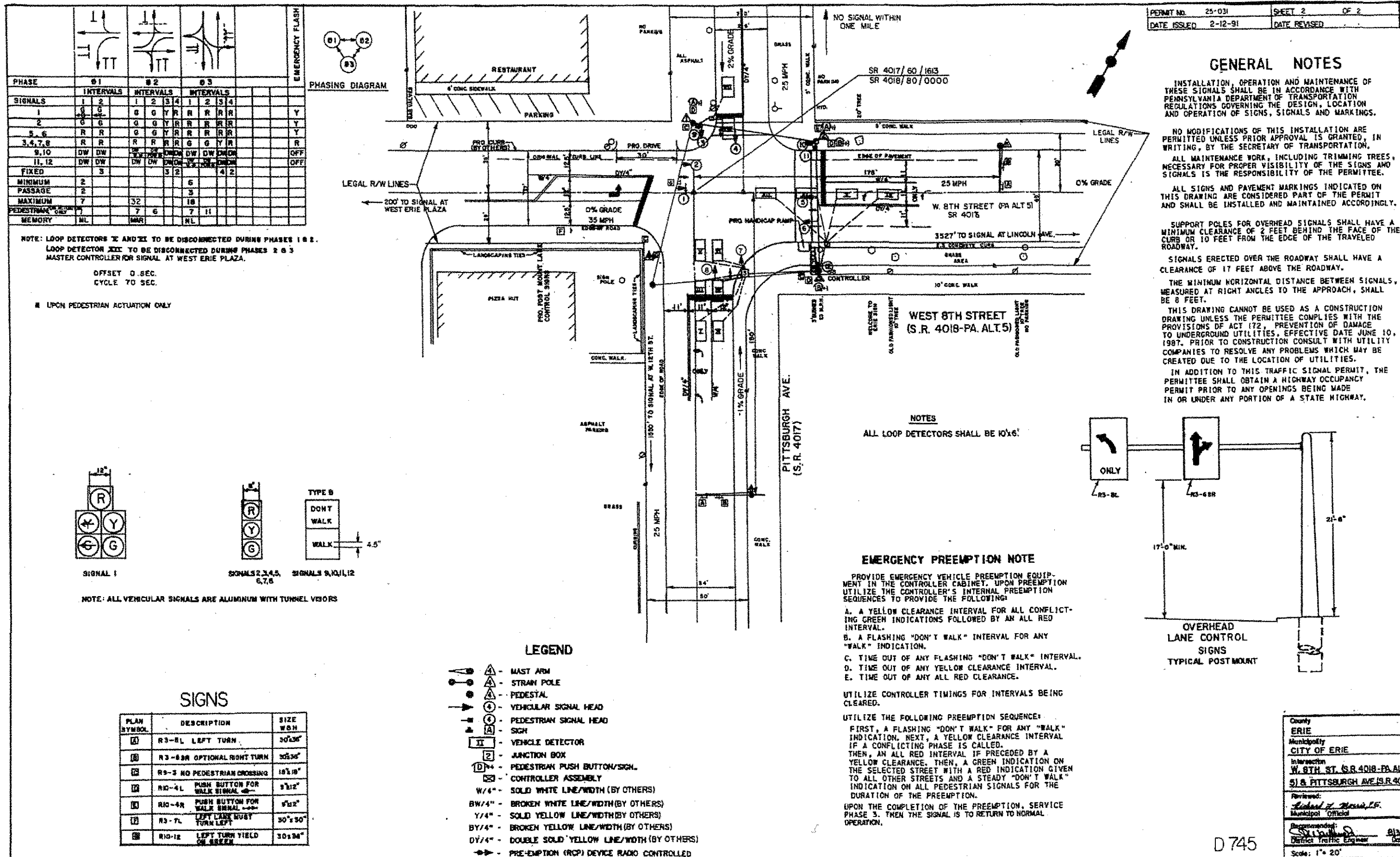


SR 4017 (Pittsburgh Ave) and West Erie Plaza





#170



PERMIT NO.	25-031	SHEET	2	OF	2
DATE ISSUED	2-12-91	DATE REVISED			

**GENERAL NOTES**

INSTALLATION, OPERATION AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION AND OPERATION OF SIGNS, SIGNALS AND MARKINGS.

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION.

ALL MAINTENANCE WORK, INCLUDING TRIMMING TREES, NECESSARY FOR PROPER VISIBILITY OF THE SIGNS AND SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAYEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED ACCORDINGLY.

SUPPORT POLES FOR OVERHEAD SIGNALS SHALL HAVE A MINIMUM CLEARANCE OF 2 FEET BEHIND THE FACE OF THE CURB OR 10 FEET FROM THE EDGE OF THE TRAVELED ROADWAY.

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A CLEARANCE OF 17 FEET ABOVE THE ROADWAY.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS, MEASURED AT RIGHT ANGLES TO THE APPROACH, SHALL BE 8 FEET.

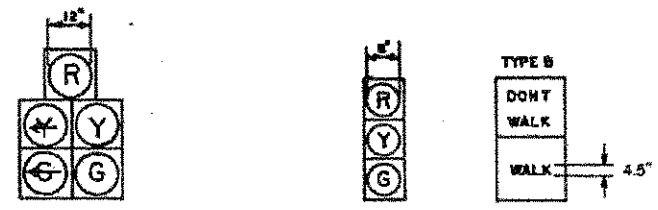
THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF ACT 172, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, EFFECTIVE DATE JUNE 10, 1987. PRIOR TO CONSTRUCTION CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

IN ADDITION TO THIS TRAFFIC SIGNAL PERMIT, THE PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT PRIOR TO ANY OPENINGS BEING MADE IN OR UNDER ANY PORTION OF A STATE HIGHWAY.

PHASE	S1				S2				S3				EMERGENCY FLASH
	1	2	3	4	1	2	3	4	1	2	3	4	
SIGNALS	1	2	3	4	1	2	3	4	1	2	3	4	
1	G	G	G	Y	R	R	R	R	R	R	R	R	Y
2	G	G	G	Y	R	R	R	R	R	R	R	R	Y
3, 4, 7, 8	R	R	R	R	G	G	G	Y	R	R	R	R	Y
9, 10	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	OFF
11, 12	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	OFF
FIXED	2	3			3	2			4	2			
MINIMUM PASSAGE	2								6				
MAXIMUM	7								3				
PEDESTRIAN													
MEMORY	NL				MR				NL				

NOTE: LOOP DETECTORS X AND XI TO BE DISCONNECTED DURING PHASES 1 & 2.  
 LOOP DETECTOR XII TO BE DISCONNECTED DURING PHASES 2 & 3  
 MASTER CONTROLLER FOR SIGNAL AT WEST ERIE PLAZA.  
 OFFSET 0 SEC.  
 CYCLE 70 SEC.

■ UPON PEDCESTRIAN ACTUATION ONLY



SIGNAL 1  
 SIGNALS 2, 3, 4, 5, 6, 7, 8  
 SIGNALS 9, 10, 11, 12

NOTE: ALL VEHICULAR SIGNALS ARE ALUMINUM WITH TUNNEL VIDEOS

**SIGNS**

PLAN SYMBOL	DESCRIPTION	SIZE W x H
(R3-BL)	R3-BL LEFT TURN	30x36"
(R3-8R)	R3-8R OPTIONAL RIGHT TURN	30x36"
(R9-3)	R9-3 NO PEDESTRIAN CROSSING	18x18"
(R10-4L)	PUSH BUTTON FOR WALK SIGNAL	9x12"
(R10-4R)	PUSH BUTTON FOR WALK SIGNAL	9x12"
(R3-7L)	LEFT LANE MUST TURN LEFT	30x30"
(R10-12)	LEFT TURN YIELD ON GREEN	30x36"

**LEGEND**

- ▲ - MAST ARM
- - STRAIN POLE
- - PEDESTAL
- ⊕ - VEHICULAR SIGNAL HEAD
- ⊙ - PEDESTRIAN SIGNAL HEAD
- ⊖ - SIGN
- ⊞ - VEHICLE DETECTOR
- ⊠ - JUNCTION BOX
- ⊡ - PEDESTRIAN PUSH BUTTON/SIGN.
- ⊞ - CONTROLLER ASSEMBLY
- W/4" - SOLID WHITE LINE/WIDTH (BY OTHERS)
- BW/4" - BROKEN WHITE LINE/WIDTH (BY OTHERS)
- Y/4" - SOLID YELLOW LINE/WIDTH (BY OTHERS)
- BY/4" - BROKEN YELLOW LINE/WIDTH (BY OTHERS)
- DY/4" - DOUBLE SOLID YELLOW LINE/WIDTH (BY OTHERS)
- ⊞ - PRE-EMPTION (RCP) DEVICE RADIO CONTROLLED

NOTES  
 ALL LOOP DETECTORS SHALL BE 10'x6'.

**EMERGENCY PREEMPTION NOTE**

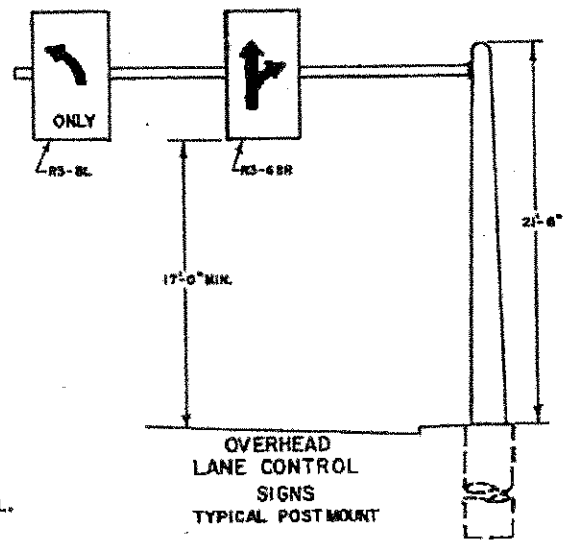
PROVIDE EMERGENCY VEHICLE PREEMPTION EQUIPMENT IN THE CONTROLLER CABINET. UPON PREEMPTION UTILIZE THE CONTROLLER'S INTERNAL PREEMPTION SEQUENCES TO PROVIDE THE FOLLOWING:

- A YELLOW CLEARANCE INTERVAL FOR ALL CONFLICTING GREEN INDICATIONS FOLLOWED BY AN ALL RED INTERVAL.
- A FLASHING "DON'T WALK" INTERVAL FOR ANY "WALK" INDICATION.
- TIME OUT OF ANY FLASHING "DON'T WALK" INTERVAL.
- TIME OUT OF ANY YELLOW CLEARANCE INTERVAL.
- TIME OUT OF ANY ALL RED CLEARANCE.

UTILIZE CONTROLLER TIMINGS FOR INTERVALS BEING CLEARED.

UTILIZE THE FOLLOWING PREEMPTION SEQUENCE:  
 FIRST, A FLASHING "DON'T WALK" FOR ANY "WALK" INDICATION. NEXT, A YELLOW CLEARANCE INTERVAL IF A CONFLICTING PHASE IS CALLED. THEN, AN ALL RED INTERVAL IF PRECEDED BY A YELLOW CLEARANCE. THEN, A GREEN INDICATION ON THE SELECTED STREET WITH A RED INDICATION GIVEN TO ALL OTHER STREETS AND A STEADY "DON'T WALK" INDICATION ON ALL PEDESTRIAN SIGNALS FOR THE DURATION OF THE PREEMPTION.

UPON THE COMPLETION OF THE PREEMPTION, SERVICE PHASE 3. THEN THE SIGNAL IS TO RETURN TO NORMAL OPERATION.



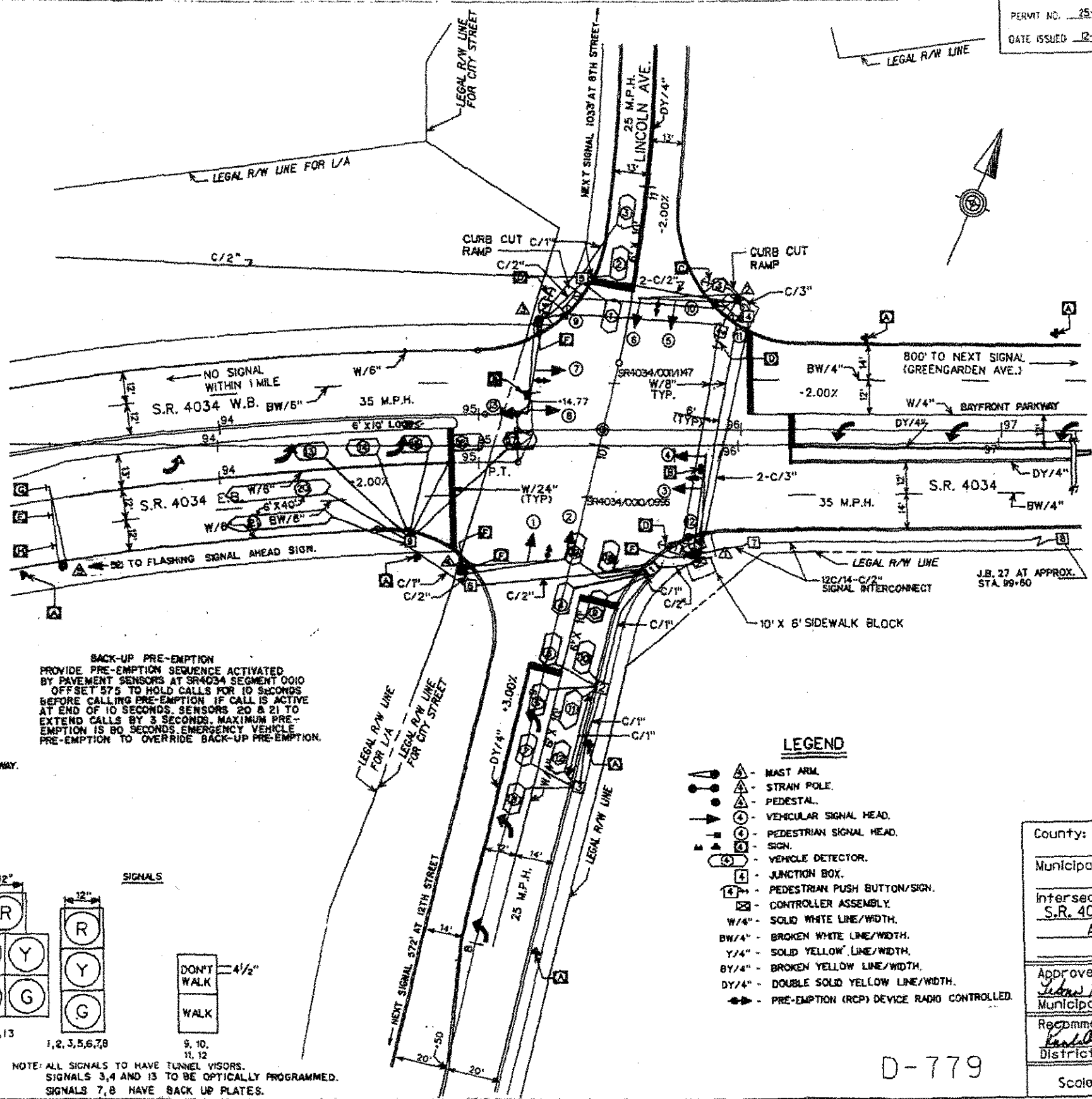
OVERHEAD LANE CONTROL SIGNS TYPICAL POST MOUNT

County	ERIE
Municipality	CITY OF ERIE
Intersection	W. 8TH ST. (S.R. 4018-PA ALT 5) & PITTSBURGH AVE (S.R. 4017)
Reviewed:	Richard J. Harris, P.E. Municipal Official
Recommended:	Blaine Cox District Traffic Engineer
Scale:	1" = 20'

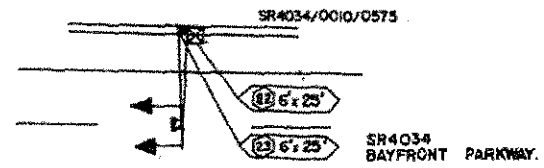
D 745

#171

PERMIT NO. 25-045 SHEET 2 OF 3  
 DATE ISSUED 12-15-97 DATE REVISED 1-17-08



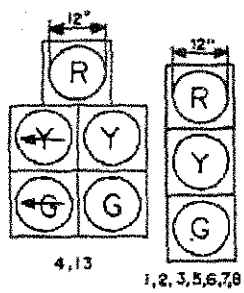
FLASHING SIGNAL AHEAD SIGN SEE PERMIT # F25-019.



**BACK-UP PRE-EMPTION**  
 PROVIDE PRE-EMPTION SEQUENCE ACTIVATED BY PAVEMENT SENSORS AT SR4034 SEGMENT 0010 OFFSET 575 TO HOLD CALLS FOR 10 SECONDS BEFORE CALLING PRE-EMPTION IF CALL IS ACTIVE AT END OF 10 SECONDS. SENSORS 20 & 21 TO EXTEND CALLS BY 3 SECONDS. MAXIMUM PRE-EMPTION IS 80 SECONDS. EMERGENCY VEHICLE PRE-EMPTION TO OVERRIDE BACK-UP PRE-EMPTION.

**SIGNING**

PLAN SYMBOL	DESCRIPTION	SIZE
R3-7L	LEFT LANE MUST TURN LEFT	30"x30"
R10-12	LEFT TURN YIELD ON GREEN	30"x36"
R10-4R	PUSH BUTTON FOR WALK SIGNAL	9"x12"
R10-4L	PUSH BUTTON FOR WALK SIGNAL	9"x12"
R3-5S	STRAIGHT THROUGH	30"x36"
R9-3	NO PEDESTRIAN CROSSING	18"x18"
R3-5L	LEFT TURN	30"x36"
R3-6SR	OPTIONAL RIGHT TURN	30"x36"



NOTE: ALL SIGNALS TO HAVE TUNNEL VISORS.  
 SIGNALS 3, 4 AND 13 TO BE OPTICALLY PROGRAMMED.  
 SIGNALS 7, 8 HAVE BACK UP PLATES.

- LEGEND**
- ▲ - MAST ARM
  - △ - STRAIN POLE
  - - PEDESTAL
  - ⊙ - VEHICULAR SIGNAL HEAD
  - ⊙ - PEDESTRIAN SIGNAL HEAD
  - ⊙ - SIGN
  - ⊙ - VEHICLE DETECTOR
  - ⊙ - JUNCTION BOX
  - ⊙ - PEDESTRIAN PUSH BUTTON/SIGN.
  - ⊙ - CONTROLLER ASSEMBLY
  - W/4" - SOLID WHITE LINE/WIDTH.
  - BW/4" - BROKEN WHITE LINE/WIDTH.
  - Y/4" - SOLID YELLOW LINE/WIDTH.
  - BY/4" - BROKEN YELLOW LINE/WIDTH.
  - DY/4" - DOUBLE SOLID YELLOW LINE/WIDTH.
  - ⊙ - PRE-EMPTION (RCP) DEVICE RADIO CONTROLLED.

County: ERIE  
 Municipality: CITY OF ERIE  
 Intersection: S.R. 4034 (BAYFRONT PARKWAY) AND LINCOLN AVENUE

Approved and Accepted:  
John Parnick / Date  
 Municipal Official

Recommended:  
Keith A. B... / Date  
 District Traffic Engineer

Scale: 1" = 10 FEET

D-779

PLOTTER: 31-JUL-2007 08:54

OPERATOR: C.W. FILE NAME: 012-302-199

#173

PERMIT NO. 25-048 SHEET 2 OF 3  
DATE ISSUED 12-15-97 DATE REVISED 5-9-07

**GENERAL NOTES**

INSTALLATION, OPERATION, AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION, AND OPERATION OF SIGNALS, SIGNALS, AND PAVEMENT MARKINGS.

NO MODIFICATIONS OF THE INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION OR HIS REPRESENTATIVE.

MAINTENANCE WORK ON ALL APPROACHES TO THE SIGNALIZED INTERSECTION NECESSARY FOR PROPER VISIBILITY OF THE SIGN AND SIGNALS, INCLUDING TRIMMING OF TREES, IS THE RESPONSIBILITY OF THE PERMITTEE.

UNLESS OTHERWISE NOTED, THE PERMITTEE SHALL MAINTAIN CURBING AND OTHER DRAINAGE FACILITIES IN ACCORDANCE WITH THE DEPARTMENT STANDARDS AND CRITERIA, EXCEPT FOR INLET GRATES THAT WILL BE MAINTAINED BY THE DEPARTMENT AS A PART OF THE ROADWAY SURFACE.

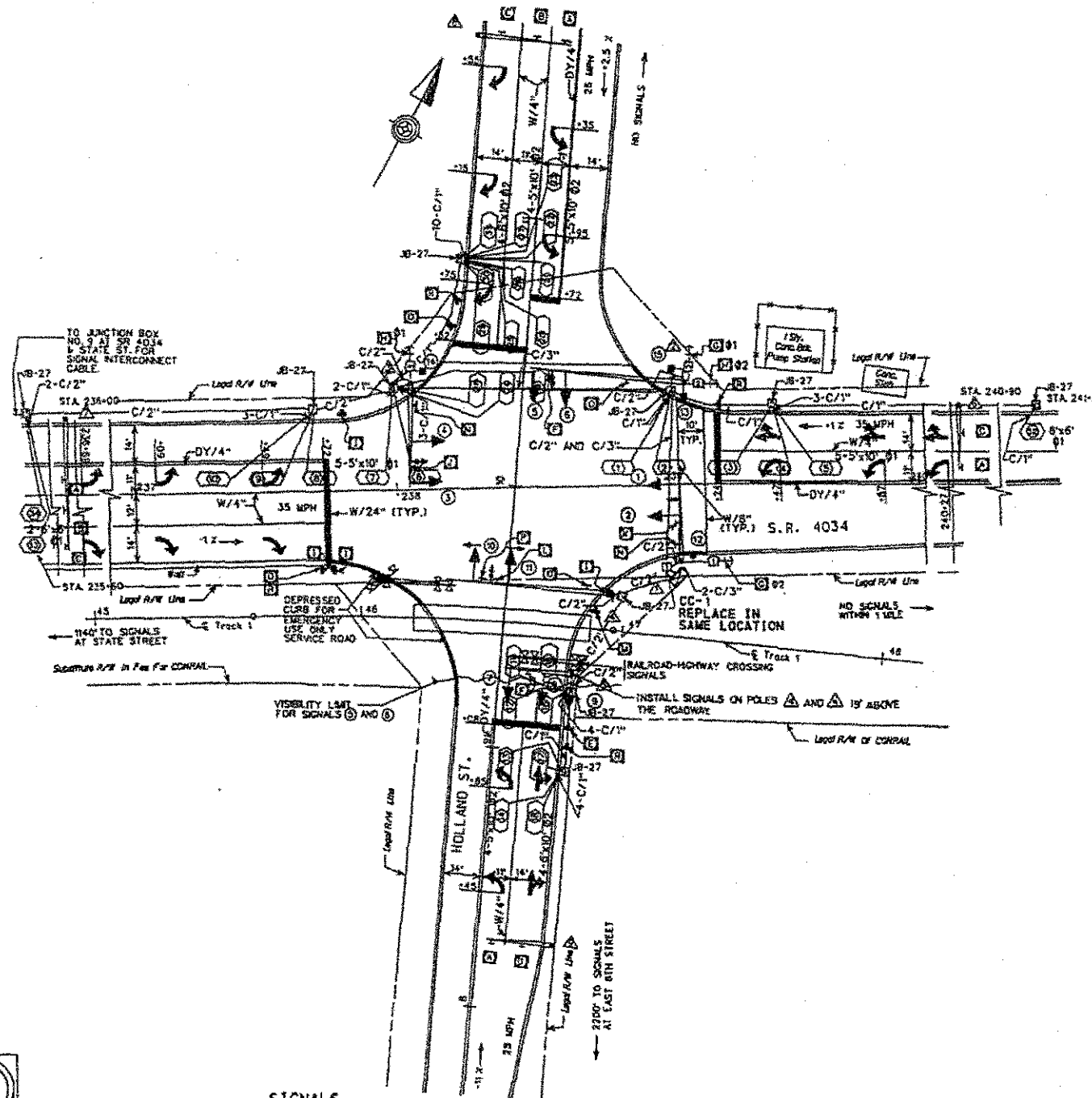
ALL SIGNS, CURB RAMPS, SIDEWALKS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED BY THE PERMITTEE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION PUBLICATION 68, OFFICIAL TRAFFIC-CONTROL DEVICES. EXCEPTIONS ARE LONGITUDINAL PAVEMENT MARKINGS ON STATE HIGHWAYS, WHICH ARE MAINTAINED BY THE DEPARTMENT.

SUPPORT POLES FOR OVERHEAD SIGNALS AND SIGNS SHALL HAVE A MINIMUM CLEARANCE OF 2 FEET BEHIND THE FACE OF THE CURB OR 12 FEET FROM THE EDGE OF THE TRAVELLED ROADWAY TO EDGE OF SUPPORT POLE.

VEHICULAR SIGNALS OR SIGNS ERECTED OVER THE ROADWAY SHALL HAVE A CLEARANCE OF 17 FEET ABOVE THE ROADWAY; TETHER WIRE SHALL HAVE A CLEARANCE OF 16 FEET ABOVE ANY PORTION OF THE TRAVELLED ROADWAY. UNLESS OTHERWISE NOTED, POST MOUNTED VEHICULAR SIGNALS SHALL HAVE A CLEARANCE OF NOT LESS THAN 8 FEET NOR MORE THAN 15 FEET ABOVE THE SIDEWALK LEVEL OR PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALK EXISTS. UNLESS OTHERWISE NOTED, PEDESTRIAN SIGNALS SHALL HAVE A CLEARANCE OF 8 FEET ABOVE THE SIDEWALK LEVEL. UNLESS OTHERWISE NOTED, POST MOUNTED SIGNS SHALL HAVE A CLEARANCE OF 7 FEET ABOVE THE SIDEWALK LEVEL OF ABOVE THE EDGE OF ROADWAY GRADE, IF NO SIDEWALK EXISTS. UNLESS OTHERWISE NOTED, THE TOP OF THE POST MOUNTED SIGNS SHALL BE A MINIMUM 9 FEET ABOVE GROUND LINE.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS, MEASURED AT RIGHT ANGLES TO THE APPROACH, SHALL BE 8 FEET.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISION OF THE LATEST ACT PREVENTING DAMAGE TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS THAT MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.



**LEGEND**

- ▲ MAST ARM
- STRAIN POLE
- PEDESTAL
- ▲ VEHICULAR SIGNAL HEAD
- ▲ PEDESTRIAN SIGNAL HEAD
- SIGN
- VEHICLE DETECTOR
- JUNCTION BOX
- ▲ PEDESTRIAN PUSH BUTTON/SIGN
- CONTROLLER ASSEMBLY
- W/4" SOLID WHITE LINE/WIDTH
- BW/4" BROKEN WHITE LINE/WIDTH
- Y/4" SOLID YELLOW LINE/WIDTH
- BY/4" BROKEN YELLOW LINE/WIDTH
- DY/4" DOUBLE SOLID YELLOW LINE/WIDTH
- PRE-EMPTION (RCP) DEVICE RADIO CONTROLLED

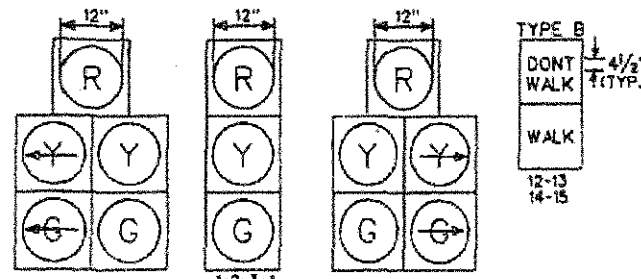
PLAN SYMBOL	DESCRIPTION	SIZE W x H
A	R3-SL LEFT TURN SIGN	30" x 36"
B	R3-S5 STRAIGHT THROUGH SIGN	30" x 36"
C	R3-SR RIGHT TURN SIGN	30" x 36"
D	R3-6SR OPTIONAL RIGHT TURN SIGN	30" x 36"
E	R10-8L STOP HERE ON RED	24" x 30"
F	R10-11 NO TURN ON RED	30" x 36"
G	R10-4R PUSH BUTTON FOR WALK SIGNAL	9" x 12"
H	R10-4L PUSH BUTTON FOR WALK SIGNAL	9" x 12"
I	R9-3 NO PEDESTRIAN CROSSING	18" x 18"
J	R3-2 NO LEFT TURN	30" x 30"
K	R3-1 NO RIGHT TURN	30" x 30"
L	R5-1 DO NOT ENTER	30" x 30"
M	R8-8 DO NOT STOP ON TRACKS	24" x 30"
N	D3-2 HOLLAND ST.	72" x 12"
O	D3-2 BAYFRONT PARKWAY	72" x 12"
P	R10-12 LEFT TURN YIELD ON GREEN	30" x 36"
Q	R3-7R RIGHT LANE MUST TURN RIGHT	30" x 30"
R	R3-7L LEFT LANE MUST TURN LEFT	30" x 30"

**SIGNS**



INTERNALLY ILLUMINATED BLANKOUT SIGNS

**SIGNALS**



ALL SIGNALS TO BE EQUIPPED WITH TUNNEL VISORS  
\* SIGNALS 5 AND 6 ARE OPTICALLY PROGRAMMED

D-791

COUNTY : ERIE

MUNICIPALITY : CITY OF ERIE

INTERSECTION : SR 4034 (BAYFRONT PARKWAY)  
AND HOLLAND STREET

APPROVED AND ACCEPTED BY :  
[Signature] DATE 5/6/07

MUNICIPAL OFFICIAL

RECOMMENDED BY :  
[Signature] DATE 5/6/07

DISTRICT TRAFFIC ENGINEER

SCALE : 25 0 25 50

OPERATOR: 2713 FILE NAME: ... \NRESIGN\MSTN254034013\_11005

3/22/2007 PLOTTED: D:\D12 CAD 101-90 REVISED 110-DU

#174

PERMIT NO. 025-064 SHEET 2 OF 3  
 DATE ISSUED 10/30/08 DATE REVISED 8-20-04

**GENERAL NOTES**

INSTALLATION, OPERATION, AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION, AND OPERATION OF SIGNS, SIGNALS, AND PAVEMENT MARKINGS.

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION OR HIS REPRESENTATIVE.

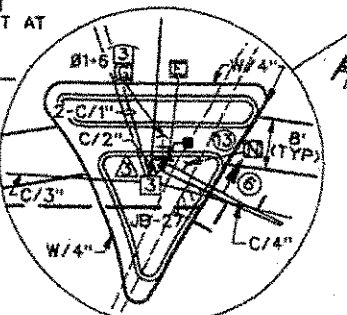
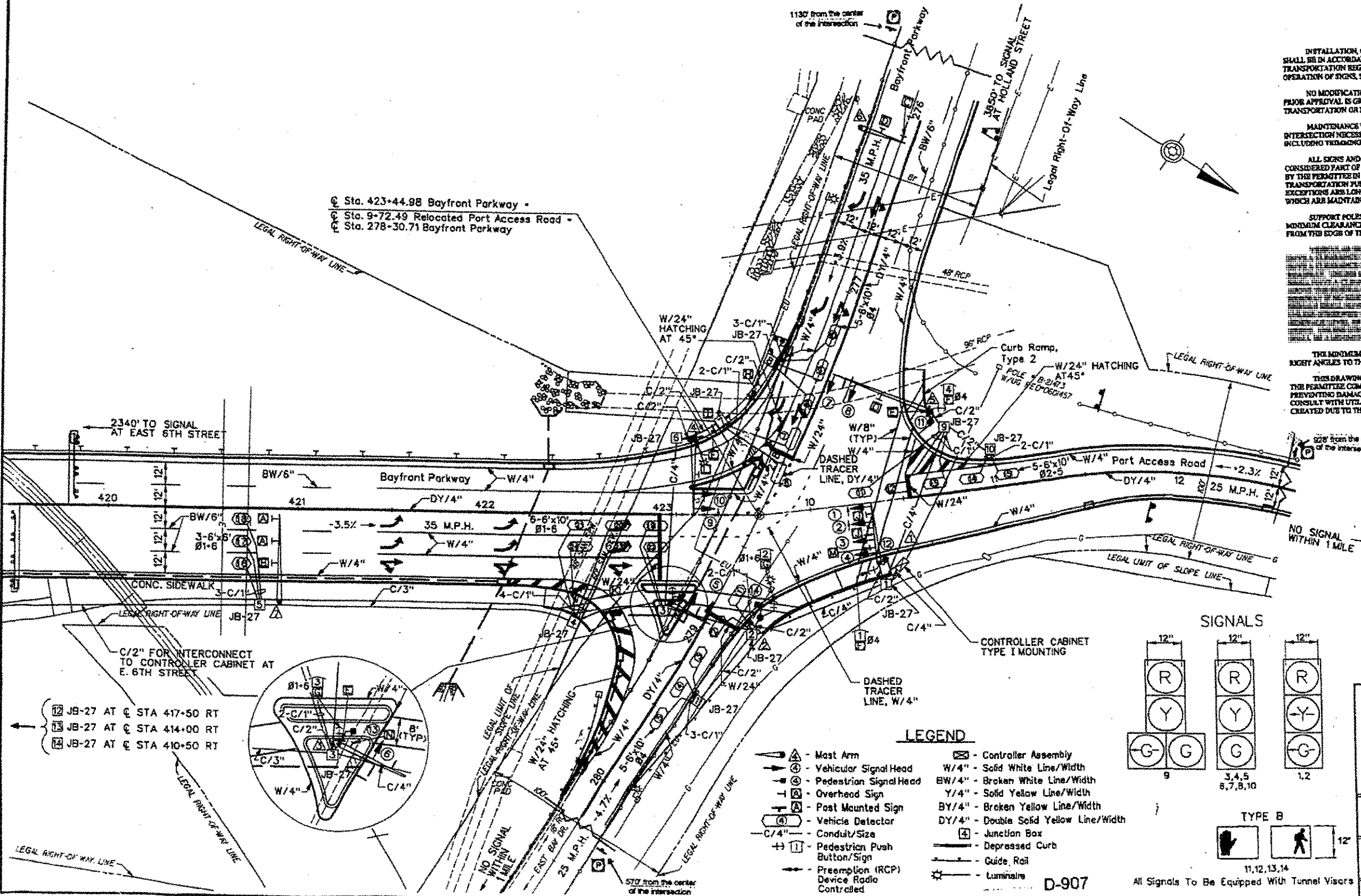
MAINTENANCE WORK ON ALL APPROACHES TO THE SIGNALIZED INTERSECTION NECESSARY FOR PROPER VISIBILITY OF THE SIGNS AND SIGNALS, INCLUDING TRIMMING OF TREES, IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED BY THE PERMITTEE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION PUBLICATION 61, OFFICIAL TRAFFIC CONTROL DEVICES. EXCEPTIONS ARE LONGITUDINAL PAVEMENT MARKINGS ON STATE HIGHWAYS, WHICH ARE MAINTAINED BY THE DEPARTMENT.

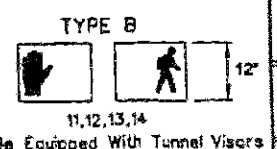
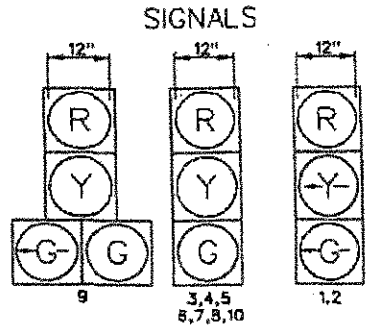
SUPPORT POLES FOR OVERHEAD SIGNALS AND SIGNS SHALL HAVE A MINIMUM CLEARANCE OF 2 FEET ABOVE THE FACE OF THE CURB OR 12 FEET FROM THE EDGE OF THE TRAVELED ROADWAY TO EDGE OF SUPPORT POLE.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS, MEASURED AT RIGHT ANGLES TO THE APPROACH, SHALL BE 4 FEET.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST ACT PREVENTING DAMAGE TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS THAT MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.



- LEGEND**
- ▲ Mast Arm
  - ⊙ Vehicular Signal Head
  - ⊙ Pedestrian Signal Head
  - ⊙ Overhead Sign
  - ⊙ Post Mounted Sign
  - ⊙ Vehicle Detector
  - ⊙ Conduit/Size
  - ⊙ Pedestrian Push Button/Sign
  - ⊙ Preemption (RCP) Device Radio Controlled
  - ⊙ Controller Assembly
  - W/4" - Solid White Line/Width
  - BW/4" - Broken White Line/Width
  - Y/4" - Solid Yellow Line/Width
  - BY/4" - Broken Yellow Line/Width
  - DY/4" - Double Solid Yellow Line/Width
  - ⊙ Junction Box
  - ⊙ Depressed Curb
  - ⊙ Guide Rail
  - ⊙ Luminaires



County: Erie  
 Municipality: City of Erie  
 Intersection: Part Access Road and East Bay Drive at Bayfront Parkway

Reviewed: *Steve J. Ransley* 11/1/08  
 Municipal Official Date

Recommended: *[Signature]* 7-30-08  
 District Traffic Engineer Date

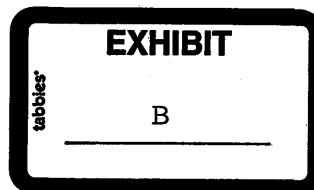
Scale: 0 25 50 75'

D-907 All Signals To Be Equipped With Tunnel Visors

City Of Meadville  
Curb Ramp Inventory

12-14-11, Revised 3-22-13, 4-4-13  
DATA - S. McMasters; Revised by P.A. Miller

Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Alley (Yes or No) "T" or Plus	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	COMMENTS
1	CRAWFORD	6	0540/0000	ARCH ST										Y	0	0		Signal Warrant. Equal access for all users provided on 2 of the 4 legs of the signalized intersection. Barrier to remain on the north and west legs in accordance with the signal permit. Prohibited legs and associated accessibility feature (ramps, ped push buttons, ped heads, etc.) will be evaluated through a future engineering study and the warranted features will be installed in a future project that addresses the need for signal upgrades and/or replacements. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable (to be constructed by the city). See Photos.
	CRAWFORD	6	0541/0000	ARCH ST										Y	0	0		
2	CRAWFORD	North Street*	N/A	WATER STREET										Y	6		NW and NE / SW and SE / NE and SE	SR 27-A02 Project. *North Street and Water are both local roads. Tee Intersection.
3	CRAWFORD	North Street*	N/A	MULBERRY STREET							Y	2	0				SW and SE	SR 27-A02 Project. *North Street and Mulberry Street are both local roads. Local access provided across Mulberry Street. Crossings not provided for North street due to proximity to Water Street and Mulberry Street is an alley without stop control on North Street. Field viewed on May 29, 2012 with VFI; proposed ramp locations acceptable.
4	CRAWFORD	North Street*	N/A	MARKET STREET										Y	8		NW and NE / SW and SE / NE and SE / NW and SW	SR 27-A02 Project. *North Street and Market Street are both local roads.
5	CRAWFORD	27	0010/0292	N. COTTAGE ST				Y		2							NW and SW	Driveway. SR 27-A02 Project. Existing driveway on the east side is a barrier for crossing SR 27. Proposed crossing on the west side of SR 27. Proposed local crossings for N. Cottage Street. Field viewed on May 29, 2012 with VFI; proposed ramp locations acceptable.
									4								NW and NE / SW and SE	
6	CRAWFORD	27	0010/0644	MAIN ST										Y		6	NW and SW / NE and SE / NW and NE	SR 27-A02 Project, anticipated completion 2014.
															2		SW and SE	
7	CRAWFORD	27	0010/0896	GARDEN ST	Y		2										NE and SE	Driveway. SR 27-A02 Project, anticipated completion 2014. Existing driveway on the west side is a barrier for crossing SR 27. Proposed crossing on the east side of SR 27. Proposed local crossings for Garden Street. Field viewed on May 29, 2012 with VFI; proposed ramp locations acceptable.
						2											NW and NE	
8	CRAWFORD	27	0010/1016	CHANCERY	Y		2										NE and SE	Driveway. SR 27-A02 Project, anticipated completion 2014. Existing driveway on the west side is a barrier for crossing SR 27. Proposed crossing on the east side of SR 27. Proposed local crossings for Chancery Lane. Field viewed on May 29, 2012 with VFI; proposed ramp locations acceptable.
						2											SW and SE	
9	CRAWFORD	27	0010/1301	LIBERTY										Y		6	NW and SW / NE and SE / SW and SE	SR 27 A02 Project, anticipated completion 2014.
															2		NW and NE	



City Of Meadville  
Curb Ramp Inventory

12-14-11, Revised 3-22-13, 4-4-13  
DATA - S. McMasters; Revised by P.A. Miller

Intersection #	County	State Route	Seg/Off Intersection	Intersecting Local Street Name	Non-Signalized "T" Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Non-Signalized Plus Intersection (Yes or No)	# of Local Ramps	# of State Ramps	Alley (Yes or No) "T" or Plus	# of Local Ramps	# of State Ramps	Signalized (Yes or No)	# of Local Ramps	# of State Ramps	Location (quadrant)	COMMENTS
10	CRAWFORD	27	0010/1547	EAST ST	Y	2											NE and SE	Driveway. SR 27-A02 Project, anticipated completion 2014. Existing driveway on the west side is a barrier for crossing SR 27. Proposed crossing on the east side of SR 27. Proposed local crossings for East Street. Field viewed on May 29, 2012 with VFI; proposed ramp locations acceptable.
						2											SW and SE	
11	CRAWFORD	27	0020/0000	NORTH ST	Y	2											NE and SE	Driveway. SR 27-A02 Project, anticipated completion 2014. Existing driveway on the north side is a barrier for crossing SR 27. Proposed local crossing for North Street. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable. See Photos.
12	CRAWFORD	27	0020/0164	KENNEDY	Y	2											NW and SW	Driveway. SR 27-A02 Project, anticipated completion 2014. Existing driveway on the south side is a barrier for crossing SR 27. Proposed state crossing for State Street. Proposed local crossing for Kennedy Street. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable. See Photos.
						2											NW and NE	
13	CRAWFORD	27	0020/0469	MOHICAN PL	Y	2											NE and SE	Driveway. Existing driveway on the southwest quadrant is a barrier for crossing SR 27. Proposed crossing on the north side of SR 27. Proposed local crossing for Mohican Place. See Photo.
						2											NW and NE	
14	CRAWFORD	27	0020/0678	STEWART ST	Y	4											NW and SW / NE and SE	
						2											NW and NE	
15	CRAWFORD	27	0020/0829	GROVE ST	Y	4											NW and SW / NE and SE	
						2											SW and SE	
16	CRAWFORD	27	0030/0275	PENN ST	Y	4											NW and SW / NE and SE	
						2											SW and SE	
17	CRAWFORD	27	0030/1914	HARTMAN ST	Y	2											NE and SE	Driveway. Existing driveway on the west side is a barrier for crossing SR 27. Proposed crossing on the east side of SR 27. Proposed local crossing for Hartman Street. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable. See Photos.
						2											SW and SE	
18	CRAWFORD	27	0040/1286	GRAFF ST	Y	2											NW and SW	2 existing compliant ramps
						2											NW and NE	
19	CRAWFORD	77	0010/0554	BRAWLEY AVE	Y	4											NW and SW / NE and SE	
						2											SW and SE	
20	CRAWFORD	886	0010/1150	BALDWIN ST (2037)	Y	0	0											Driveway. Existing driveway on the south side is a barrier for crossing SR 886. Existing crossing on the north side of SR 886. Existing crossing for Baldwin Street. Field viewed on May 11, 2012 with VFI; existing ramp locations acceptable. See Photos.
21	CRAWFORD	886	0010/1643	CHURCH ST				Y		4							NW and NE / SW and SE	
22	CRAWFORD	886	0010/2086	HENRY ST	Y	4											NW and SW / NE and SE	
																	NW and NE / SW and SE	

City Of Meadville  
Curb Ramp Inventory

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						2											NE and SE	
23	CRAWFORD	886	0010/2170	LOOMIS ST	Y		4										NW and NE / SW and SE	2 existing compliant local ramps. Due to the proximity of Henry street, no ramps proposed on the south side of SR 886. See photo. Added 2 state ramps per April 4, 2013 conference call with VFI.
24	CRAWFORD	886	0010/2432	SHERMAN ST	Y		2										SW and SE	4 existing compliant ramps. 1 local ramp maybe impacted with new state ramp.
25	CRAWFORD	886	0010/2831	E.COLLEGE ST	Y	1	4										SE NW and NE / SW and SE	
26	CRAWFORD	886	0010/3351	PROSPECT ST	Y	2											NE and SE NW and NE	4 existing compliant ramps.
27	CRAWFORD	886	0020/0501	ALLEGHENY ST				Y		4							SW and SE / NW and NE	Allegheny and Limber form a skewed plus intersection (8 total ramps required). 2 existing compliant local ramps at Allegheny Street.
28	CRAWFORD	886	0020/0518	LIMBER RD					2								NE and SE	
29	CRAWFORD	886	0020/1200	BYLLESBY AVE	Y		2										NW and NE	Byllesby (Tee-east) and Ben Avon (Tee-west) create an offset plus intersection. Existing compliant local ramps for Byllesby Avenue. Proposed State crossing NW and NE between Byllesby and Ben Avon. Proposed local ramp on SW quadrant of Ben Avon Street. Existing local ramp NW quadrant of Ben Avon Street. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable. TIF maybe required if running slope exceeds 8.33% for state ramp on the east side. See Photos.
30	CRAWFORD	886	0020/1242	BENAVON ST	Y	1											SW	
31	CRAWFORD	886	0020/1751	HAMILTON AVE	Y		4										SW and SE / NW and NE SE and NE	
32	CRAWFORD	886	0020/2033	CARROLL AVE	Y	2	4										SW and SE / NW and NE SW and NW	
33	CRAWFORD	886	0020/2665	SUNSET DR	Y	2	3										SW and SE / NE SE and NE	No PAR. Shoulder only in the northwest quadrant. See photo.
34	CRAWFORD	1001	0050/0925	BEN AVON ST	Y	0	0											No PAR. Pedestrian Access Route only exists on the east side of SR 1001 (Park Avenue). 2 existing local ramps. Field viewed on May 11, 2012 with VFI; existing ramp locations acceptable. See photos.
35	CRAWFORD	1001	0050/1314	WOOD ST	Y	0	0											No PAR. Pedestrian Access Route only exists on the east side of SR 1001 (Park Avenue). 2 existing compliant local ramps. Field viewed on May 11, 2012 with VFI; existing ramp locations acceptable. See photos.
36	CRAWFORD	1001	0030/0000	NORTH ST										Y		6	NW and NE / SW and SE / NE and SE NW and SW	SR 27 A02 Project. West Leg of North Street is a local road.
37	CRAWFORD	1001	0030/0601	RANDOLPH ST				Y		2							SW and SE	6 existing compliant ramps.
38	CRAWFORD	1001	0030/1334	LOCUST ST	Y		4										SW and SE / NW and NE SW and NW	
39	CRAWFORD	1001	0040/0427	LOOMIS ST	Y	2	4										SW and SE / NW and NE	2 existing compliant ramps.

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40	CRAWFORD	1001	0040/0519	SMITH ST	Y		2										SW and SE	Driveway. Existing driveway in the northeast quadrant is a barrier for crossing SR 1001. Proposed crossing on the south side of SR 1001. 2 existing compliant local ramps. See Photo.
41	CRAWFORD	1001	0040/0927	COLLEGE ST	Y		4										SW and SE / NW and NE	2 existing compliant local ramps. 1 local ramp maybe impacted with new state ramp.
42	CRAWFORD	1001	0040/1453	PROSPECT ST	Y	1	2										SW	4 existing compliant ramps.
43	CRAWFORD	1001	0040/1891	JOHN ST	Y		2										SW and SE	4 existing compliant ramps. 1 local ramp maybe impacted with new state ramp.
44	CRAWFORD	1001	0050/0000	ALLEGHENY ST		1		Y		2							SW	6 existing compliant ramps.
45	CRAWFORD	1001	0050/0621	RAVINE ST	Y		0										NW and NE	Driveways and No PAR. Existing driveways on the southwest quadrant are barriers for crossing SR 1001. No pedestrian access route in the northwest quadrant. 2 existing compliant local ramps. See Photo.
46	CRAWFORD	2012	0010/1158	ORCHARD PL	Y		4										NW and SW / NE and SE	
47	CRAWFORD	2012	0020/0000	ERNST PL	Y	2											NW and NE	Driveway. Existing driveway on the west side is a barrier for crossing SR 2012. Proposed crossing on the east side of SR 2012. Proposed local crossing for Ernst Place. Field viewed on May 11, 2012 with VFI; proposed ramp locations acceptable. See Photos.
48	CRAWFORD	2012	0020/0395	ALDEN ST	Y		2										NE and SE	4 existing compliant ramps (Porter Street Intersection adjacent to Alden and Chestnut Streets.) Local ramps on Chestnut Street (east side) local responsibility. See photo.
49	CRAWFORD	2012	0020/1389	ARCH ST	Y		2										NW and SW	Due to proximity, eastern ramps for Arch Street and western ramps for Charlton Street are the same. 10 total ramps required for Arch and Charlton Streets, 2 existing compliant ramps for SR 2012. See photo.
50	CRAWFORD	2012	0020/1451	CHARLTON ST	Y		4										NW and SW / NE and SE	See comment above.
51	CRAWFORD	2034	0110/1576	TERRACE ST		2		Y		2							SW / NE	No PAR. No pedestrian access route in northwest quadrant. 3 existing compliant ramps. See Photo.
52	CRAWFORD	2035	0040/0684	GROVE ST	Y		4		1								NE	2 existing compliant ramps.
53	CRAWFORD	2035	0040/1072	FAIRMONT AVE	Y		2										NW and SW / NE and SE	2 existing compliant ramps.
54	CRAWFORD	2035	0050/1164	CLINTON CT		2					Y		4				NE and SE	2 existing compliant ramps.
																	NW and NE / SW and SE	2 existing compliant ramps.



City Of Meadville  
Curb Ramp Inventory

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												2					NW and SW	
55	CRAWFORD	2035	0050/1854	CENTER ST							Y	4					NW and NE / SW and SE	4 existing compliant local ramps. Steps and grade differential are potential barrier for crossing on the north side. Consider either a TIF for running slope or elimination of the crossing on the north side. See photo. Local ramp may be impacted with new state ramp.
												1					NE	
56	CRAWFORD	2037	0010/0782	CHURCH ST	Y	0	0											Due to proximity of Church Street to the signalized intersection of Baldwin and Park Avenue, no additional ramps proposed for Church Street and Baldwin Street. Local crossing exists for Church Street. Field viewed on May 11, 2012 with VFI; existing ramp locations acceptable. See photos.
57	CRAWFORD	2037	0020/0097	MT HOPE ST	Y	0	0											Due to proximity of Mount Hope Street to the signalized intersection of Baldwin and Park Avenue, no additional ramps proposed for Mount Hope Street and Baldwin Street. Local crossing exists for Mount Hope Street. Field viewed on May 11, 2012 with VFI; existing ramp locations acceptable. See photos.
58	CRAWFORD	2037	0030/0000	REYNOLDS AVE	Y		4										NW and NE / SW and SE	
						2											SW and NW	
59	CRAWFORD	2037	0030/0140	W. COLLEGE ST	Y		4										NW and NE / SW and SE	
						2											NE and SE	
60	CRAWFORD	2037	0030/0295	SPRING ST (SR 2034)	Y		2										NW and NE	4 existing compliant ramps. 1 local ramp maybe impacted with new state ramp.
						1											NW	
61	CRAWFORD	2037	0030/0551	PROSPECT ST	Y		4										NW and NE / SW and SE	
						2											NE and SE	
62	CRAWFORD	2037	0030/1042	GLENWOOD AVE	Y		2										SW and SE	No Par. Driveways/Parking lots and shoulder on the north side of the intersection. See photo.
						1											SW	
						58	111		11	16	196	5	8	13	20	18	38	

Total ramps to be constructed

247

Intersections reviewed by:

Rod Fasenmyer, PennDOT Project Manager  
 Scott McMasters, PennDOT Highway Designer  
 Bob Zarecky, PennDOT ADA Coordinator  
 Paul A. Miller, P.E., PennDOT Plans Development Engineer

Locations noted reviewed by the following on May 11, 2012

Alan Dunfee, VFI  
 Rod Fasenmyer, PennDOT Project Manager  
 Scott McMasters, PennDOT Highway Designer  
 Bob Zarecky, PennDOT ADA Coordinator  
 Paul A. Miller, P.E., PennDOT Plans Development Engineer

Locations noted reviewed by the following on May 29, 2012

Alan Dunfee, VFI  
 Rick Hoffman, VFI  
 Mike McMullen, P.E., PennDOT Project Manager  
 Bob Zarecky, PennDOT ADA Coordinator

APPROACH GRADE -0.0%  
 SPEED LIMIT 40 M.P.H.  
 NEXT SIGNAL APPROXIMATELY  
 3050 FEET AT REYNOLDS ST.

**GENERAL NOTES**

INSTALLATION, OPERATION, AND MAINTENANCE OF THESE SIGNALS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION REGULATIONS GOVERNING THE DESIGN, LOCATION, AND OPERATION OF SIGNS, SIGNALS, AND PAVEMENT MARKINGS.

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED, IN WRITING, BY THE SECRETARY OF TRANSPORTATION OR HIS REPRESENTATIVE.

MAINTENANCE WORK ON ALL APPROACHES TO THE SIGNALIZED INTERSECTION NECESSARY FOR PROPER VISIBILITY OF THE SIGNS AND SIGNALS, INCLUDING TRIMMING OF TREES, IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED BY THE PERMITTEE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION PUBLICATION 68, OFFICIAL TRAFFIC-CONTROL DEVICES. EXCEPTIONS ARE LONGITUDINAL PAVEMENT MARKINGS ON STATE HIGHWAYS, WHICH ARE MAINTAINED BY THE DEPARTMENT.

SUPPORT POLES FOR OVERHEAD SIGNALS SHALL HAVE A MINIMUM CLEARANCE OF 2 FEET BEHIND THE FACE OF THE CURB OR 12 FEET FROM THE EDGE OF THE TRAVELED ROADWAY TO EDGE OF SUPPORT POLE.

VEHICULAR SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A CLEARANCE OF 17 FEET ABOVE THE ROADWAY. UNLESS OTHERWISE NOTED, POST MOUNTED VEHICULAR SIGNALS SHALL HAVE A CLEARANCE OF NOT LESS THAN 8 FEET NOR MORE THAN 15 FEET ABOVE THE SIDEWALK LEVEL OR PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALK EXISTS. UNLESS OTHERWISE NOTED, PEDESTRIAN SIGNALS SHALL HAVE A CLEARANCE OF 8 FEET ABOVE THE SIDEWALK LEVEL.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS, MEASURED AT RIGHT ANGLES TO THE APPROACH, SHALL BE 8 FEET.

THIS DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF THE LATEST ACT PREVENTING DAMAGE TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. CONSULT WITH UTILITY COMPANIES TO RESOLVE ANY PROBLEMS THAT MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

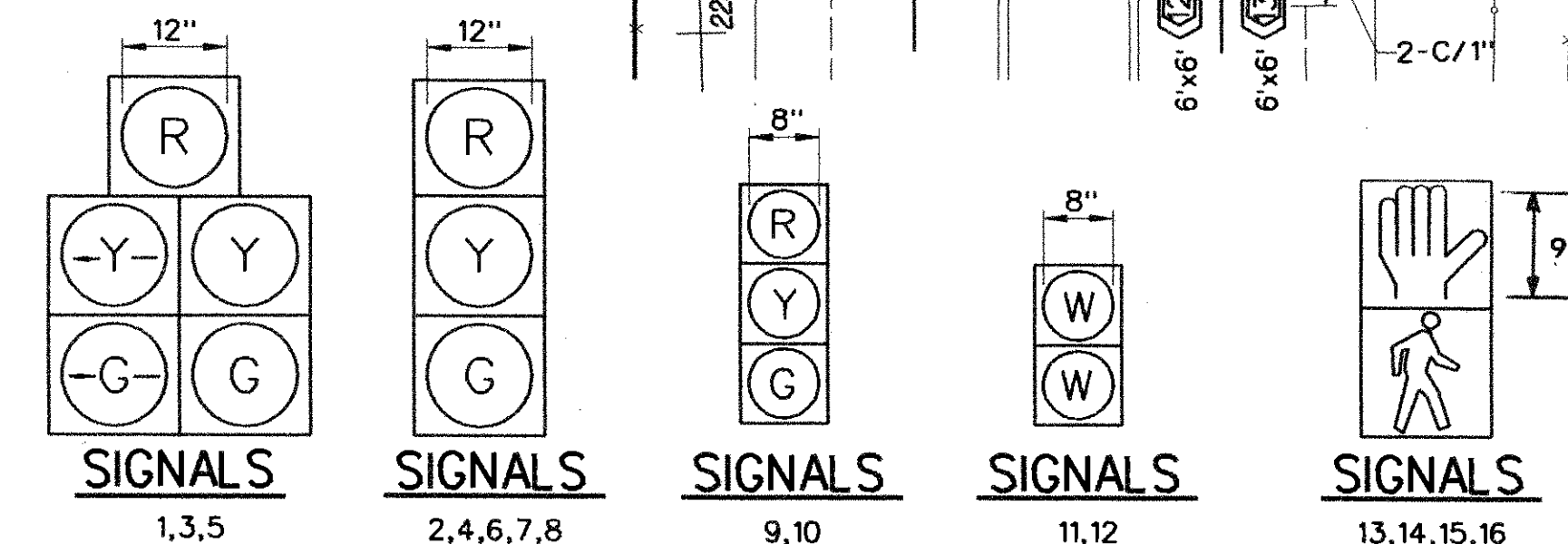
APPROACH GRADE -1.13%  
 SPEED LIMIT 25 M.P.H.  
 NEXT SIGNAL APPROXIMATELY  
 380 FEET AT WATER ST.

APPROACH GRADE -0.5%  
 SPEED LIMIT 40 M.P.H.  
 NEXT SIGNAL APPROXIMATELY  
 1889 FEET AT MERCER ST.

**SIGN TABULATION**

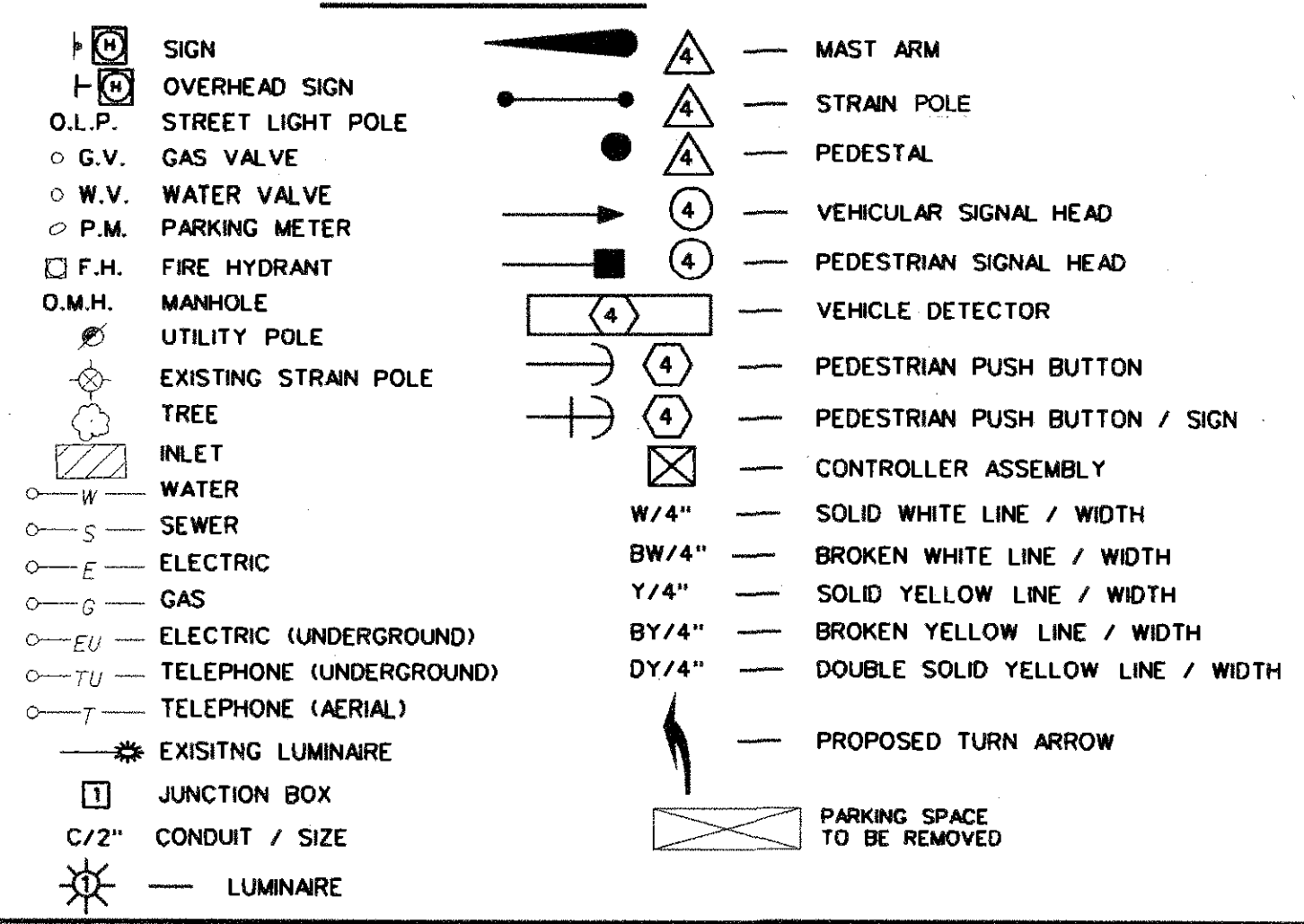
PLAN SYMBOL	DESCRIPTION	SIZE W x H	NOMENCLATURE
A	LEFT TURN ONLY	30"x36"	R3-5L
B	OPTIONAL RIGHT	30"x36"	R3-6SR
C	DO NOT ENTER	36"x36"	R5-1
D	OPPOSING TRAFFIC HAS EXTENDED GREEN	30"x36"	R10-20
E	NO PEDESTRIAN CROSSING	24"x24"	R9-3
F	NO LEFT TURN *	30"x36"	SPECIAL
G	NO RIGHT TURN *	30"x36"	SPECIAL
H	LEFT TURN ONLY *	30"x36"	SPECIAL
J	LEFT LANE MUST TURN LEFT	30"x30"	R3-7L
K	YIELD	36"x36"	R1-2
L	DOUBLE ARROW	30"x30"	W12-1
M	PUSH BUTTON FOR WALK SIGNAL ←	9"x12"	R10-4
N	PUSH BUTTON FOR WALK SIGNAL →	9"x12"	R10-4
P	LEFT TURN YIELD ON GREEN	30"x36"	R10-12
R	STOP HERE ON RED	24"x30"	R10-6L

\* INTERNALLY ILLUMINATED BLANKOUT SIGNS



SIGNALS 5,6,7,8,9,10 EQUIPPED WITH BACK PLATES  
 SIGNALS 11,12 LENS ARE LUNAR WHITE

**PLAN LEGEND**



D-237

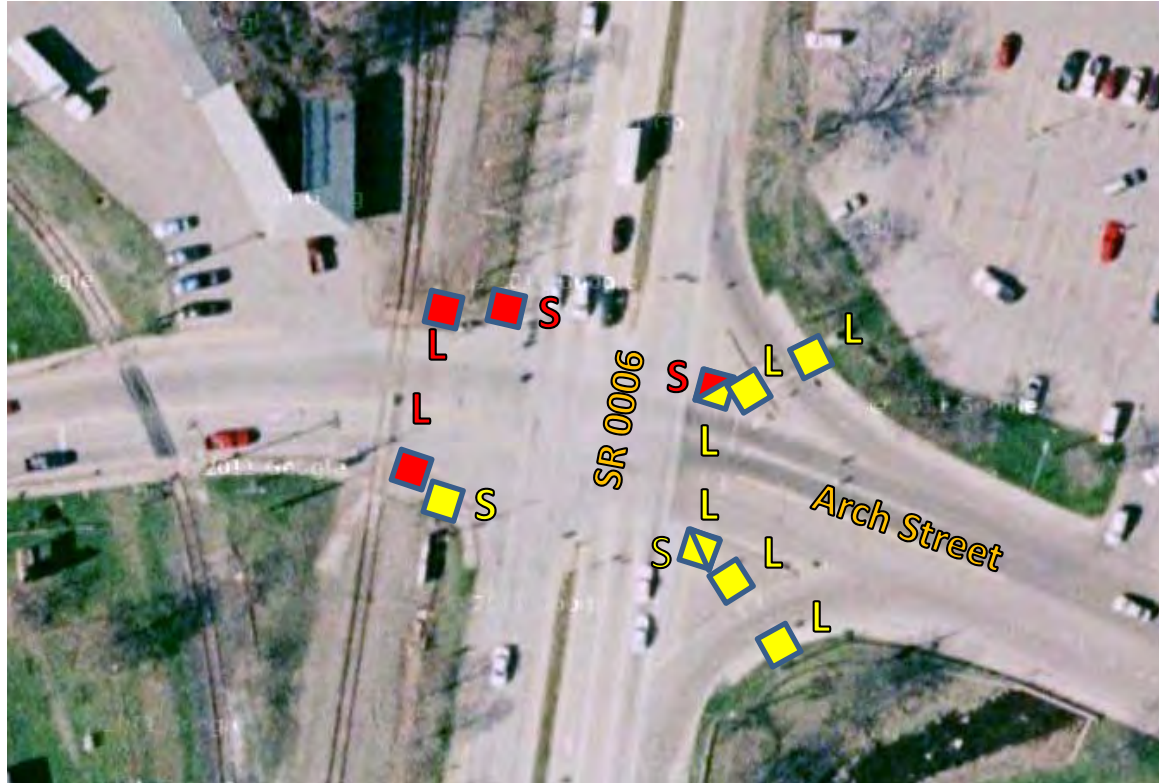
County: CRAWFORD  
 Municipality: CITY OF MEADVILLE  
 Intersection: MEAD AVENUE & FRENCH CREEK PARKWAY (S.R. 0006)

Reviewed: [Signature] 2-11-98  
 Municipal Official Date

Recommended: [Signature] 3-9-98  
 District Traffic Engineer Date

Scale: 25 0 25 50 Feet

TRACED BY \_\_\_\_\_  
 FINAL BY \_\_\_\_\_



■ Existing or proposed ramp

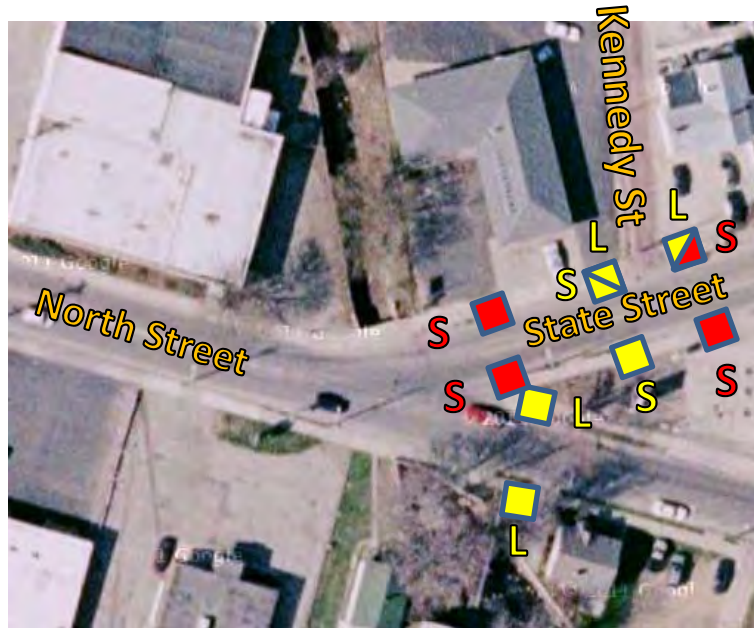
■ Barrier to remain

Arch Street & SR 6

Resolution:

Existing and proposed ramps provide desired level of access. Intersection configuration in accordance with existing signal permit.

Barriers to remain on 2 legs of the intersection because sidewalk does not exist. Remaining local ramps to be constructed by the City of Meadville.





North Street & State Street

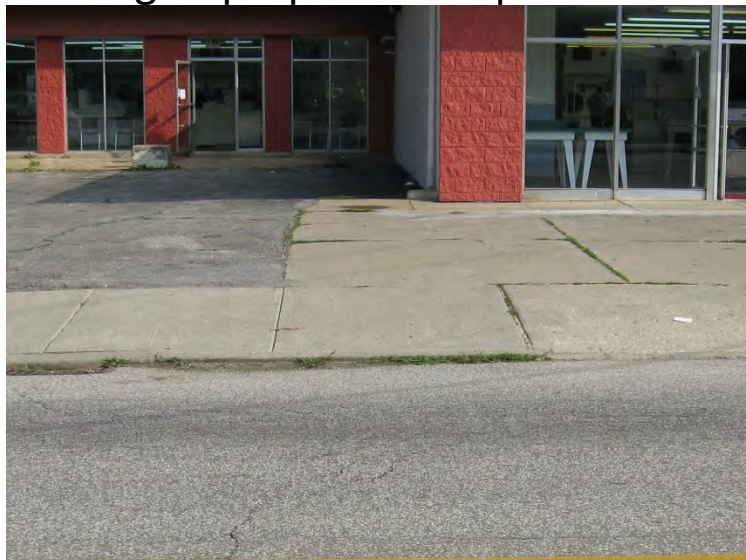
Resolution:

Proposed ramps provide desired level on connectivity

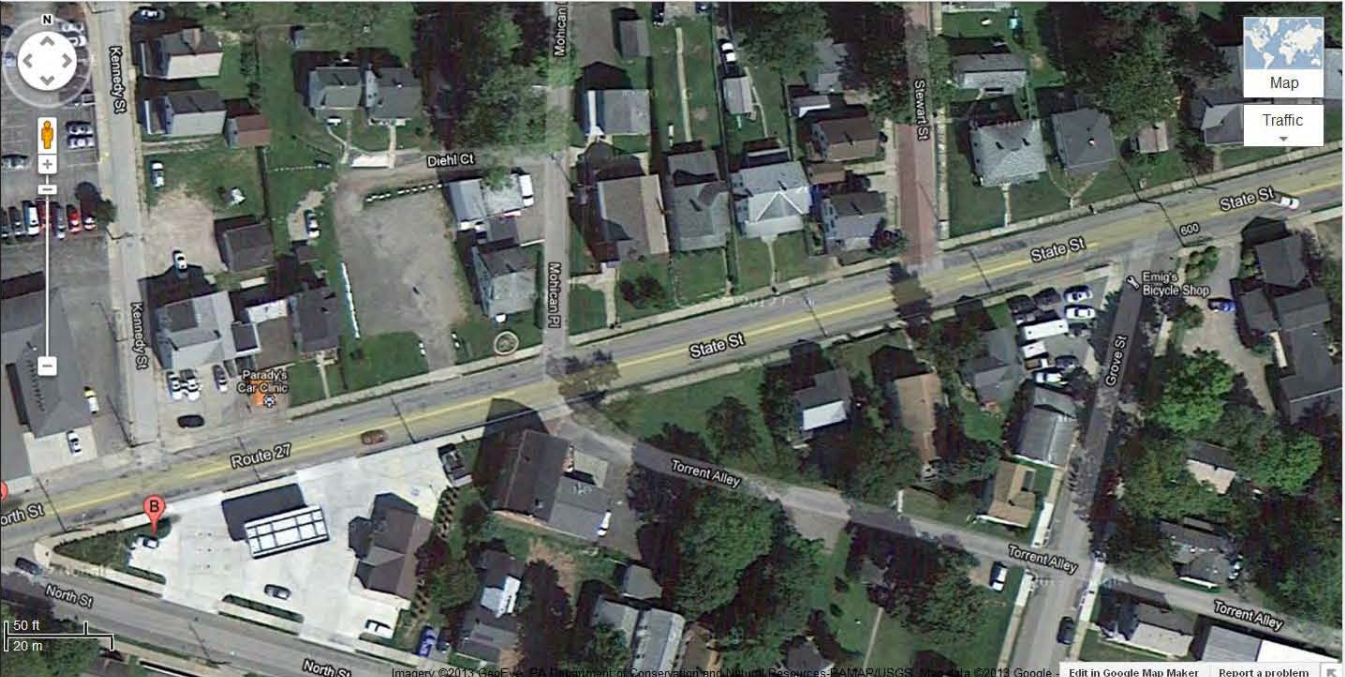
Barriers to remain on State Street due to existing driveways.

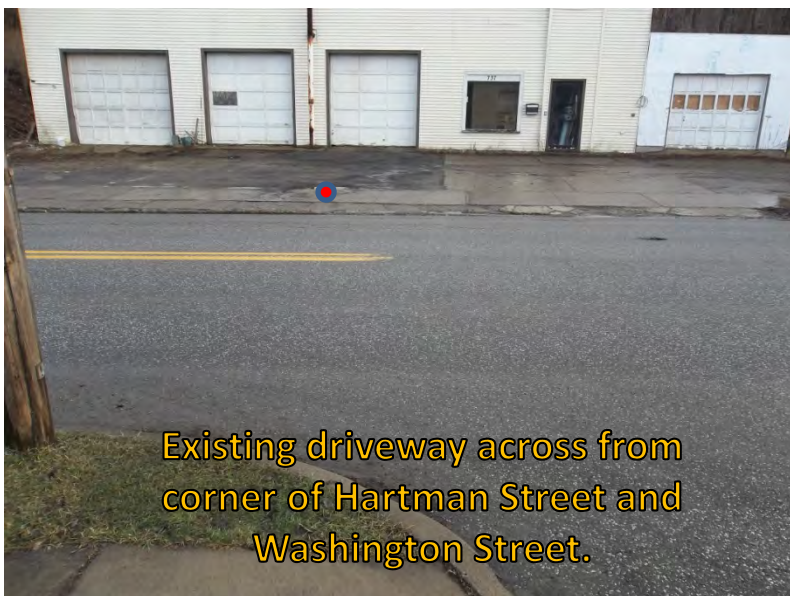
 Existing or proposed ramp

 Barrier to remain



SR 27 and Mohican Place





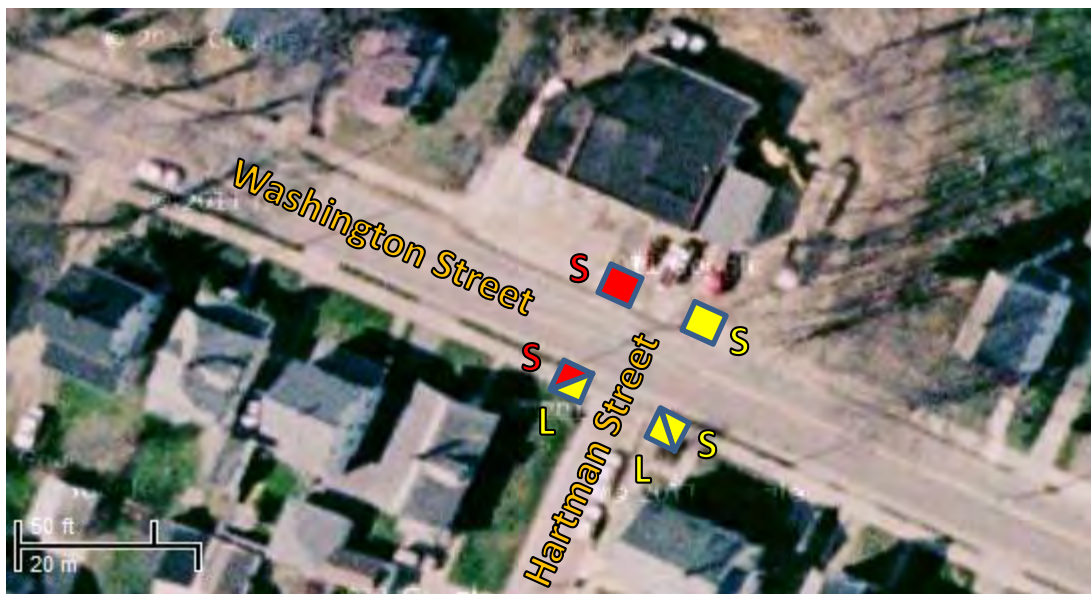
Existing driveway across from corner of Hartman Street and Washington Street.



Proposed location across from corner of Hartman Street and Washington Street. Location only area not driveway.

■ Existing or proposed ramp

■ Barrier to remain



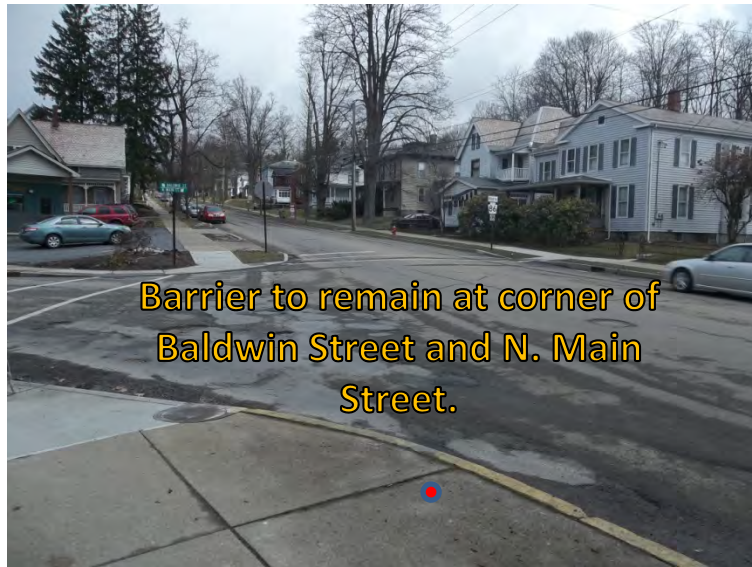
Hartman Street & Washington Street

Resolution:

Provide 2 ramps for local crossing of Hartman Street and provide 2 ramps for state crossing of Washington Street.

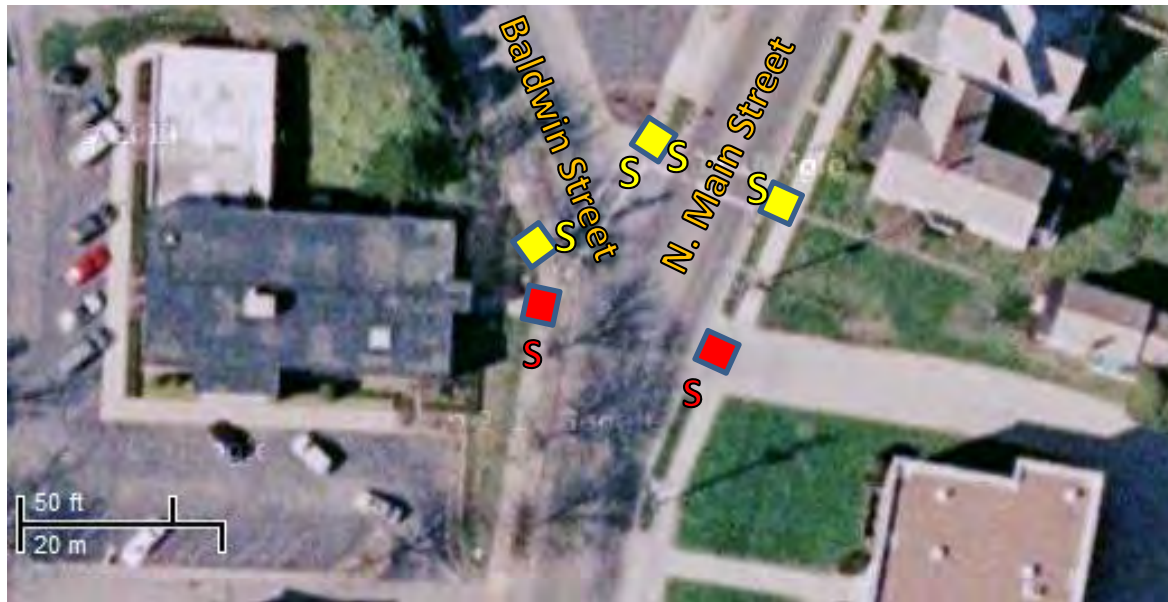
Access to be provided for 2 of the 3 legs.

Barriers to remain at 1 leg due to existing driveway.



■ Existing or proposed ramp

■ Barrier to remain



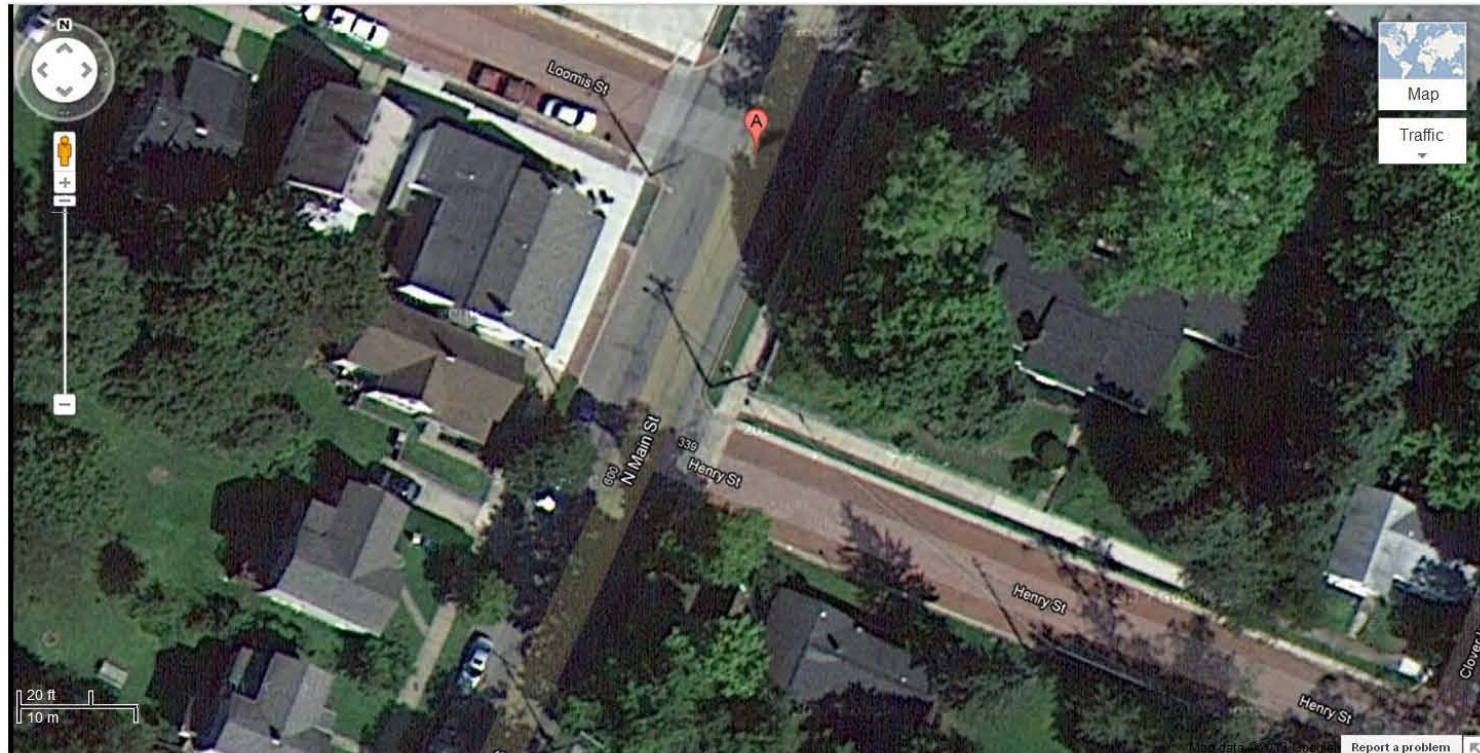
### Baldwin Street & N. Main Street

#### Resolution:

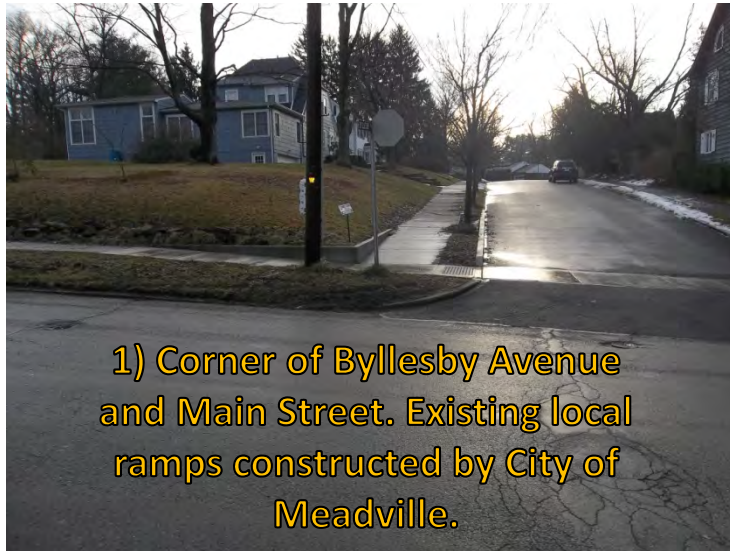
Existing ramps provide desired level of access and crossing functions adequately with school crossing guard.

Barriers to remain at leg on N. Main Street due to existing driveway and safety/sight distance concerns relating to excessive distance of stop bar from potential crossing.

SR 886 and Loomis Street







■ Existing or proposed ramp     
 ■ Barrier to remain



Ben Avon Street, Byllesby Avenue & N. Main Street

Resolution:

- 1) Corner of Byllesby Avenue and North Main: Construct ramp to cross North Main, use existing level landing area. A TIF will be required since the running slope will most likely exceed 8.33%
- 2) Corner of Ben Avon and North Main: Construct a new state and local ramp.

Access will be provided parallel to North Main (both sides). Barriers to remain due to grade differential between sidewalk and road.

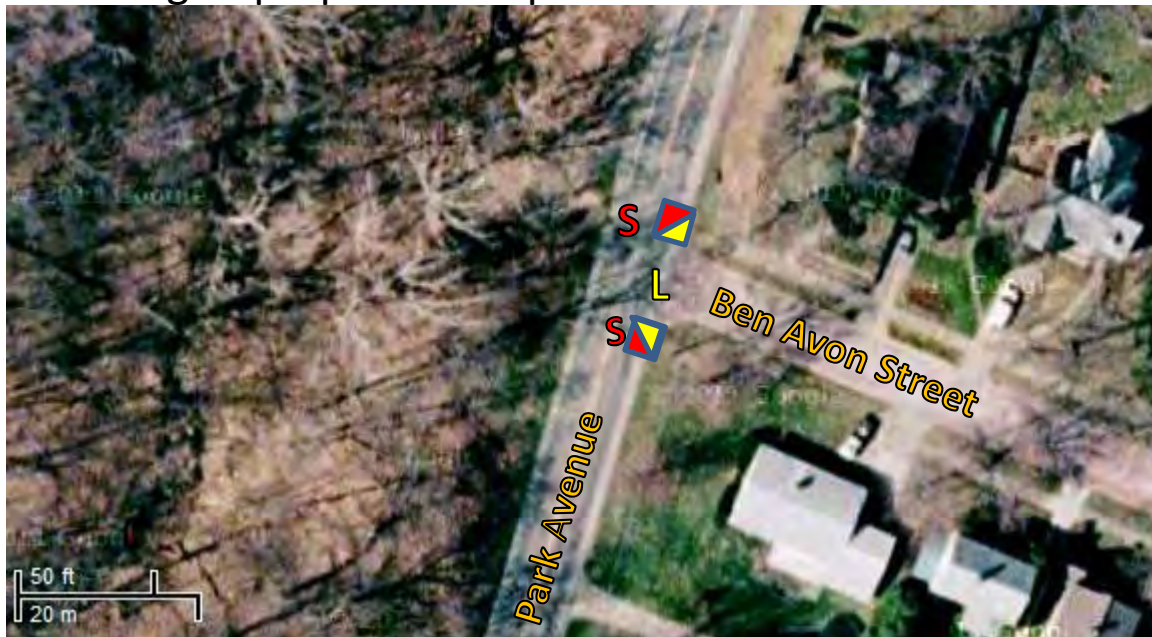
SR 886 and Sunset Drive





■ Existing or proposed ramp

■ Barrier to remain

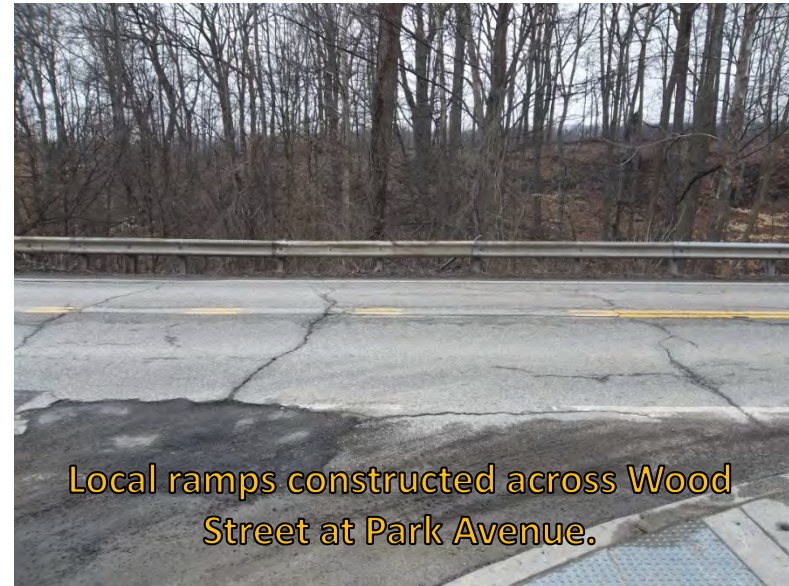


Ben Avon Street & Park Avenue

Resolution:

Existing ramps provide desired level of access.

Barriers to remain since parallel sidewalk does not exist on Park Avenue opposite of Ben Avon Street.



■ Existing or proposed ramp

■ Barrier to remain



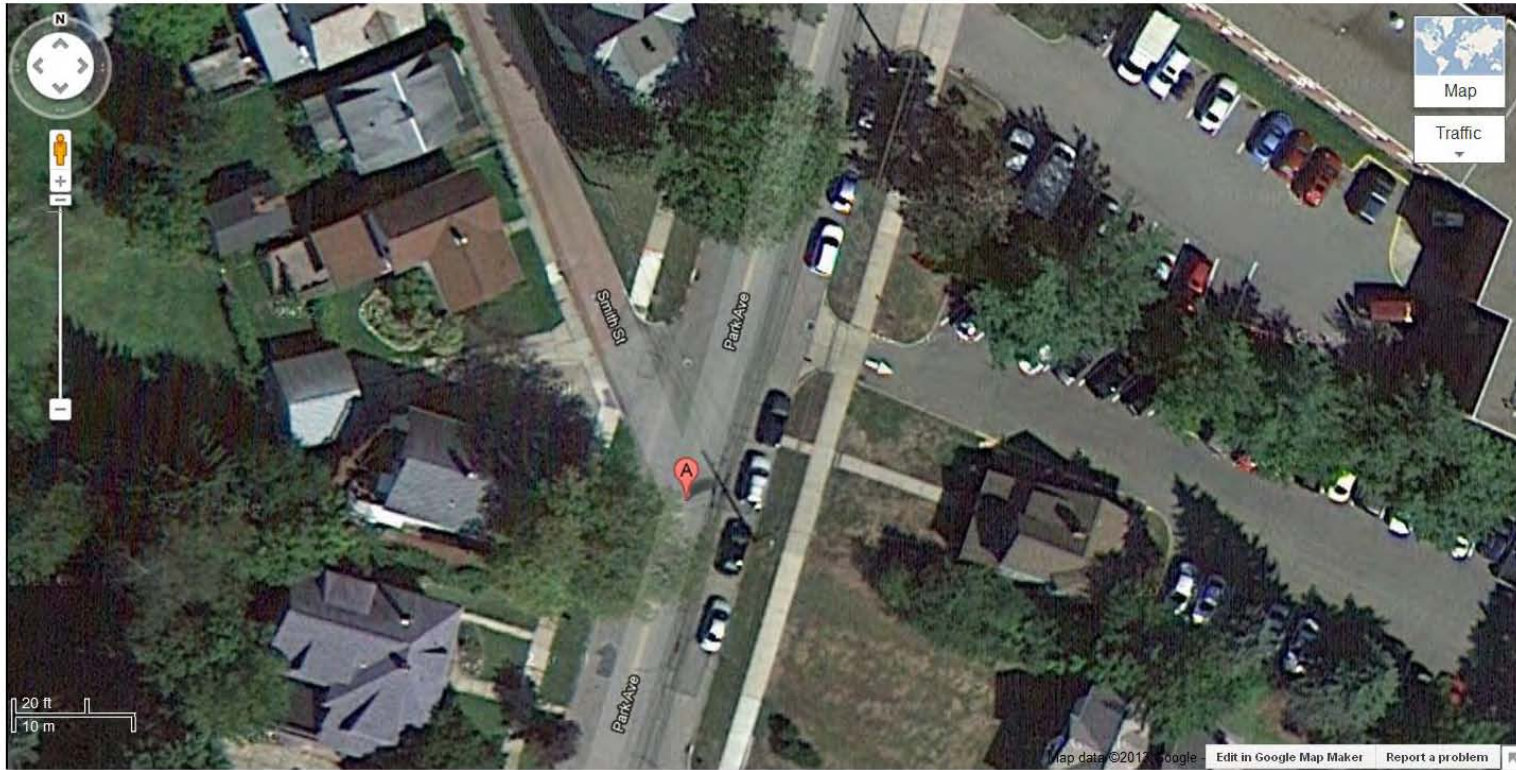
Wood Street & Park Avenue

Resolution:

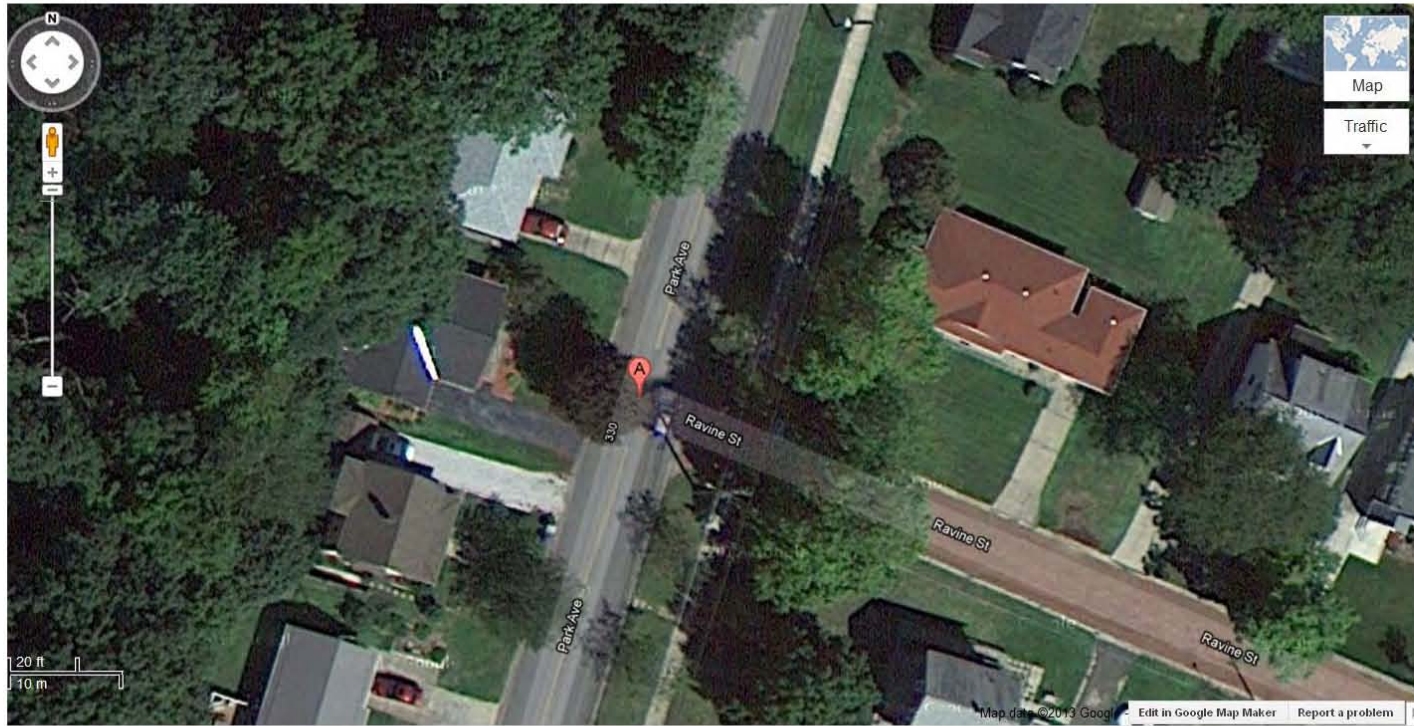
Existing ramps provide desired level of access.

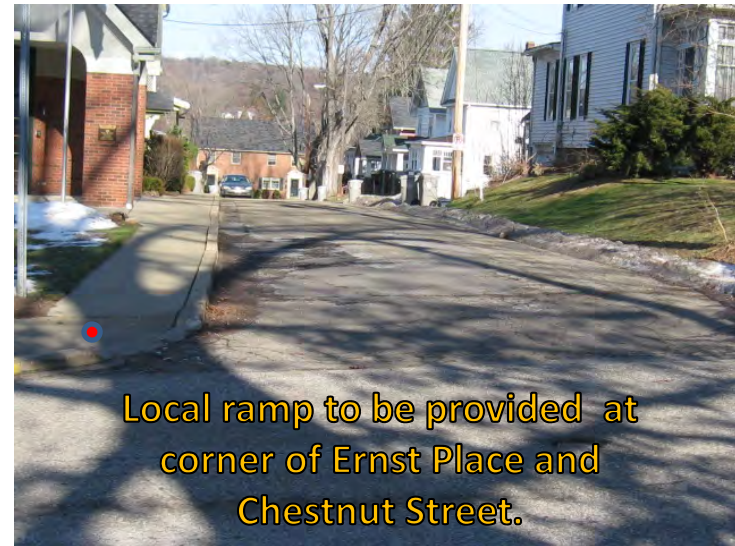
Barriers to remain since parallel sidewalk does not exist on Park Avenue opposite of Wood Street.

SR 1001 and Smith Street



SR 1001 and Ravine Street





■ Existing or proposed ramp

■ Barrier to remain



Ernst Place & Chestnut Street  
Resolution:

Provide 2 ramps for local crossing at Ernst Place and provide 2 ramps for state crossing of Chestnut Street.

Access will be provided for 2 of the 3 legs.

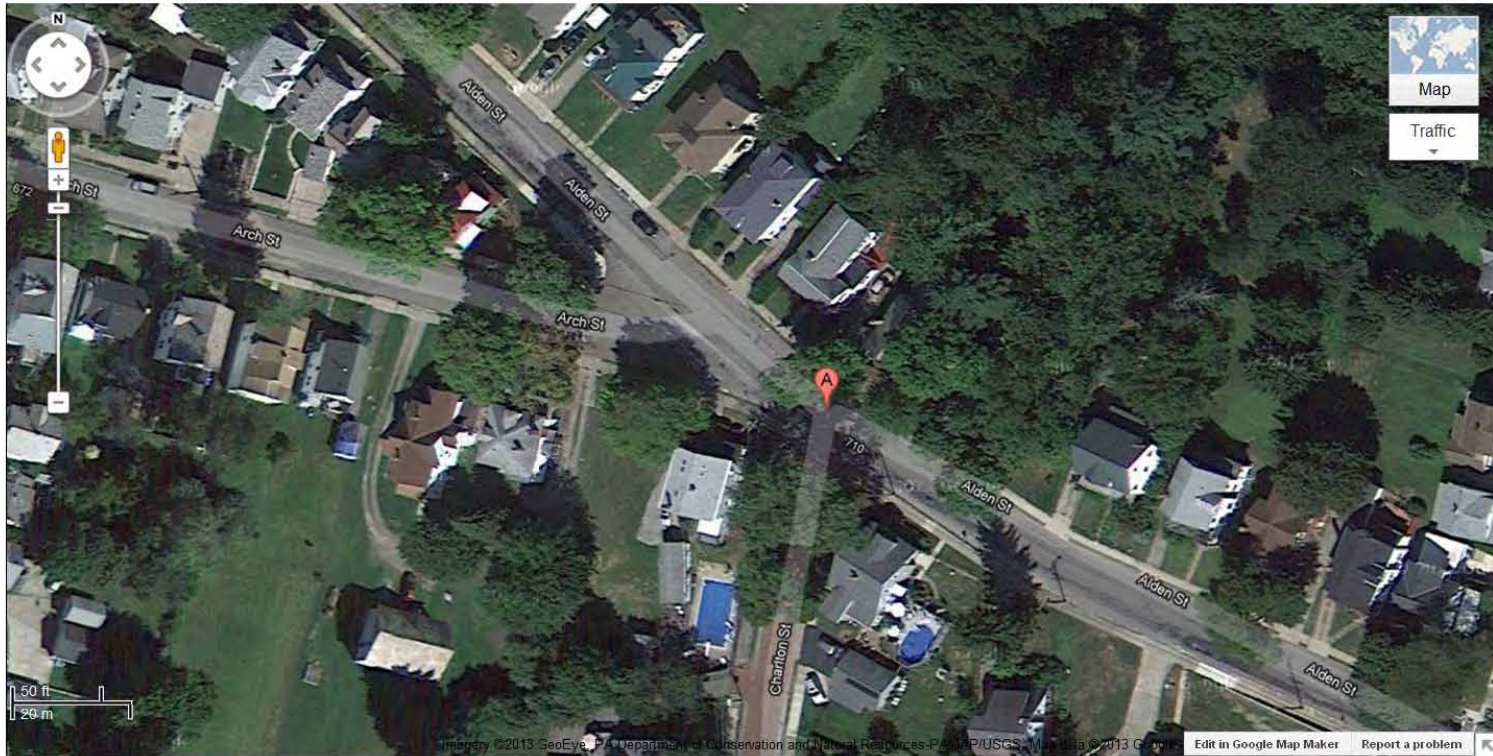
Barriers to remain at 1 leg due to existing driveway.

SR 2012 and Alden Street





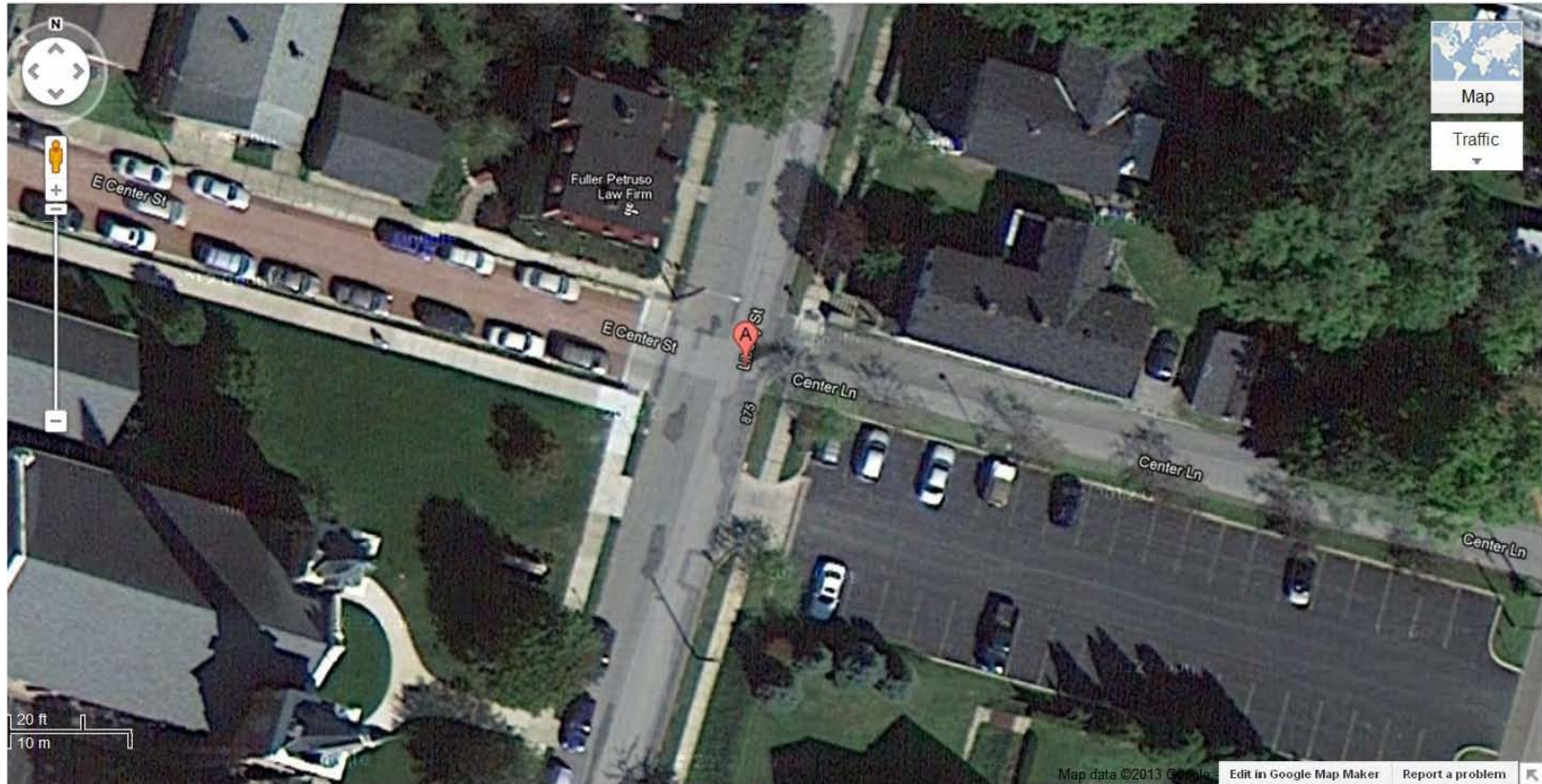
### SR 2012 and Arch Street



SR 2034 and Terrance Street

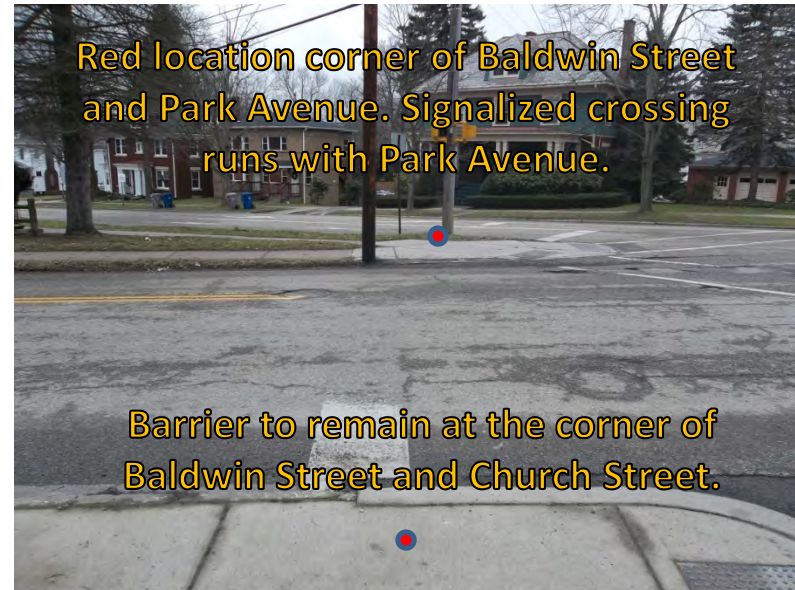


SR 2035 and Center Street





Existing ramps at the corner of Baldwin Street, Park Avenue and Church Street.



Red location corner of Baldwin Street and Park Avenue. Signalized crossing runs with Park Avenue.

Barrier to remain at the corner of Baldwin Street and Church Street.

■ Existing or proposed ramp

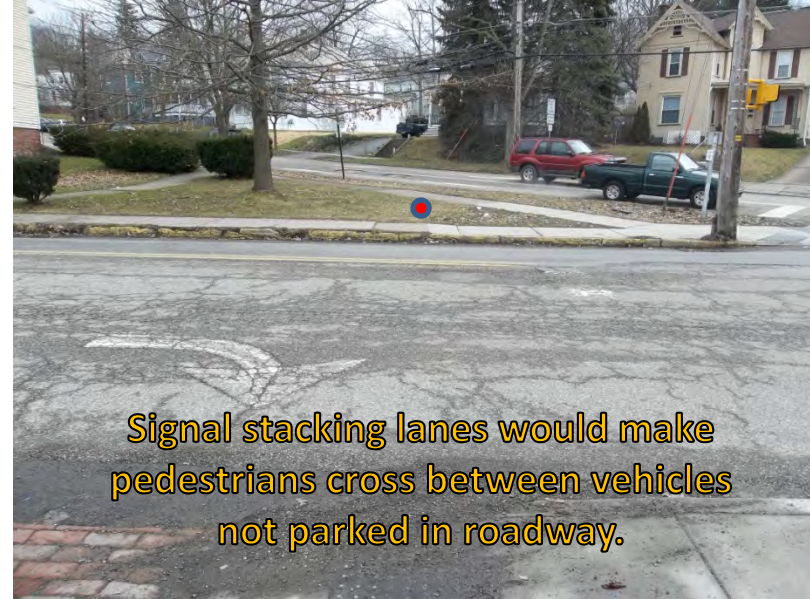
■ Barrier to remain



Church Street & Baldwin Street Resolution:

Existing ramps provide desired level of access.

Barriers to remain at perpendicular crossing of Baldwin Street due to traffic signal layout, lane configurations, and phasing.



■ Existing or proposed ramp

■ Barrier to remain

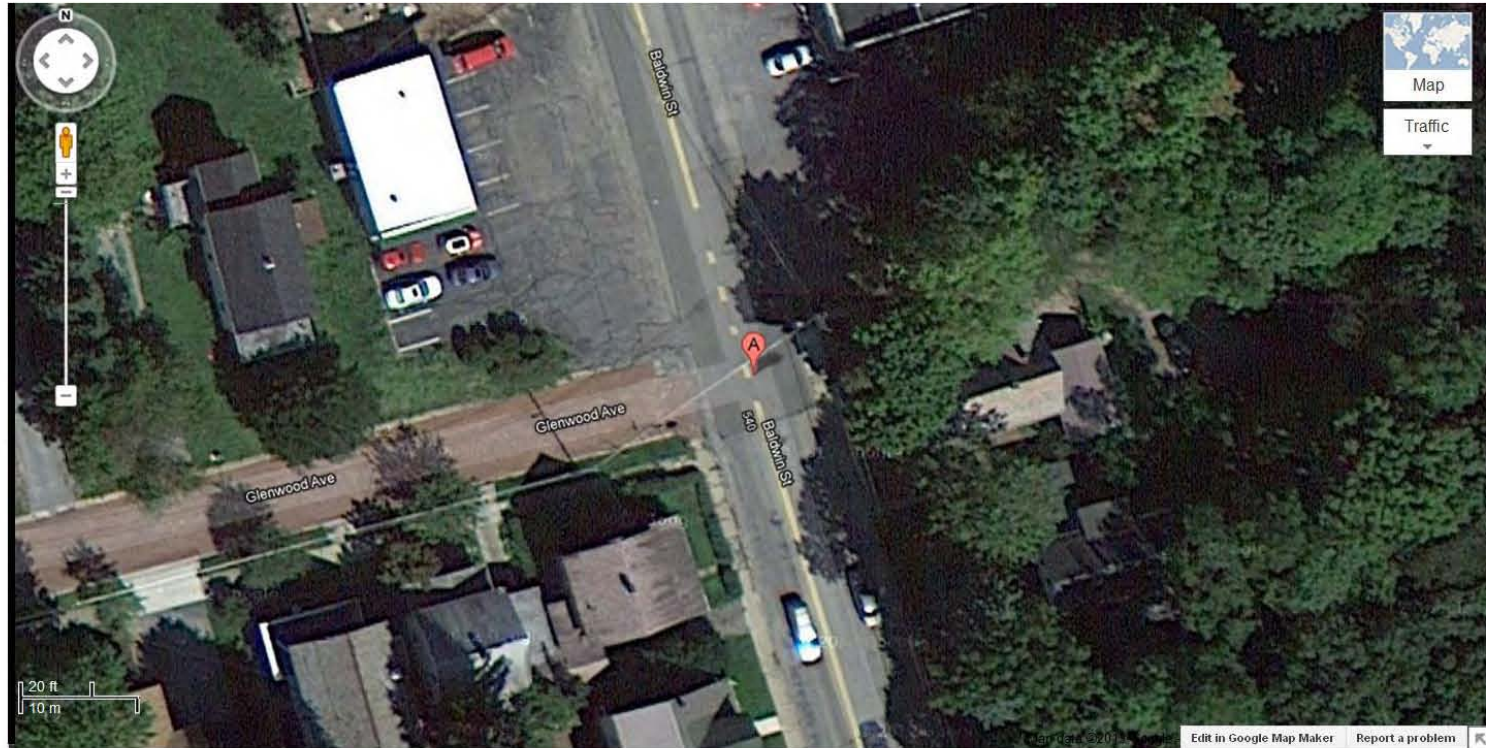


Mount Hope Street & Baldwin Street Resolution:

Existing ramps provide desired level of access.

Barriers to remain at perpendicular crossing of Baldwin Street due to traffic signal layout, lane configurations, and phasing.

SR 2037 and Glenwood Avenue



**IN THE UNITED STATES DISTRICT COURT  
FOR THE WESTERN DISTRICT OF PENNSYLVANIA**

VOICES FOR INDEPENDENCE, (VFI),	)	Civil Action No.: 06-78 Erie
MICHAEL L. EAKIN; JAY SHUFFSTALL;	)	
CAROLYN A. CREHAN; KATHY A. HERTZOG	)	
PAUL PECUNAS; SANDRA FULLER	)	HON. SEAN J. McLAUGHLIN
MARY ANN PARSNIK; PAT WEAVER;	)	
FELICIA BONGIORNO; MELVIN BORREO	)	
STEPHEN CLARK; JILL HRINDA-PATTEN	)	
MARY D. KRUG; and B. LYNNE VESTAL,	)	
on behalf of themselves and all others	)	
similarly situated	)	
	)	
Plaintiffs,	)	
	)	
v.	)	
	)	
COMMONWEALTH OF PENNSYLVANIA	)	
DEPARTMENT OF TRANSPORTATION;	)	
ALLEN D. BIEHLER, P.E., in his official	)	
capacity as Secretary of Transportation of	)	
the Commonwealth of Pennsylvania, CITY OF	)	
ERIE, PENNSYLVANIA and CITY OF	)	
MEADVILLE, PENNSYLVANIA	)	
	)	
Defendants.	)	

**ORDER**

AND NOW, upon consideration of the Fifth Settlement Agreement between Plaintiffs and Defendant, Commonwealth of Pennsylvania Department of Transportation, it is hereby ORDERED, ADJUDGED and DECREED that said Agreement is APPROVED and is hereby entered as an Order of this Court. The Court expressly retains jurisdiction to enforce the terms and conditions of the Settlement Agreement.

\_\_\_\_\_  
Honorable Sean J. McLaughlin  
United States District Judge